 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	27 July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	5	WARD: Horley East And Salfords

APPLICATION NUMBER:	21/03303/F	VALID:	17/01/2022
APPLICANT:	F74 D Chateau S.A.R.L C/O Salmon Property Horley L	AGENT:	Lambert Smith Hampton
LOCATION:	TITAN HOUSE CROSSOAK LANE SALFORDS SURREY RH1 5EX		
DESCRIPTION:	The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application was deferred from the Planning Committee meeting of 6 July. The item was deferred as the motion to grant was not carried but there were no reasons for refusal put forward at the meeting.

It is noted that concerns were raised at Committee regarding the Daylight and Sunlight study and questions were raised regarding the positioning of the building.

In terms of the positioning of the building the applicant's have been clear that the reasoning for the building being positioned at the northern end of the site adjacent to Empire Villas is in order to best protect neighbouring amenity by using the building as a mitigation measure against the noise of the activities within the site. It is of note that the Council's own environmental health consultants (RSS) agree with this approach within their consultation response of 09/02/2022 stating: "The layout of the proposed development allows the proposed building to act as a physical barrier by locating the loading bay and parking areas to the southern side of the site. In addition, no openable doors or windows (other than fire escapes) are sited on the northern elevations. This layout will act to better protect the amenity of occupiers of Empire Villas as compared to the existing site layout."

Based on this officers are of the view that this layout approach is the most appropriate in terms of protecting the amenity of the residents from noise and disturbance and it is likely that any alternative layout in terms of having the building at the opposite end of the site or turning the building 90 degrees would result in a much less acceptable relationship with the Empire Villas.

In terms of the impact on Daylight and Sunlight the first matter to stress is that under the Council's own guidance, which refers to the BRE 25 degree and 45 degree tests, the applicant has demonstrated that the scheme would pass these tests (with the only windows to fail being secondary windows at 11 Empire Villas) and therefore the Council's own guidance indicates that the proposal would not result in an unacceptable loss of light. It is normally only when a proposal fails these tests that officers would expect to see a Daylight and Sunlight Report.

The applicant did however decide to provide a Daylight and Sunlight report due to concerns at the June committee in relation to the impact on the properties in Empire Villas.

As set out in the 6 July Committee Report the Daylight and Sunlight Report considers two scenarios, 1 – excluding evergreen hedge along northern boundary and 2 – including evergreen hedge along northern boundary. Given the vegetation exists and there is no requirement for it to be removed, scenario 2 is the more appropriate for planning assessment but both are included for information. Given the concerns raised further details of the results are set out below.

In scenario 1 - the Vertical Sky Component (VSC), records 91% BRE compliance and is considered very good for the area. Windows that demonstrate transgressions beyond the BRE Guidelines (only 8 out of 89) are considered remote and the majority record very good levels of daylight availability. These windows relate to 2, 3 and 4 and 11 Empire Villas. In terms of 2 and 3 Empire Villas the analysis shows that the one window impacted at both properties form part of a group of three windows (bay window) which serve the same room. At 4 Empire Villas two windows are impacted. The technical analysis shows that both transgressions record a light change within 8% beyond the BRE's permissible 20% from former value. One of the window retains a daylight value in excess of 26% and demonstrates very good light availability. In terms of number 11, the methodology shows that 5 out of 9 windows (56%) will meet the strict application of the BRE Guidelines, i.e. 4 windows transgress. The technical analysis shows that all 4 transgressions will record retained light levels in excess of 17%. It is of note that 3 of these windows are secondary windows to rooms served by windows which do not transgress. The report concludes that the level of VSC for this dwelling far exceeds the strict application of the BRE Guidelines and is considered an indicator for good levels of light availability.

In terms of the No-Sky Line (NSL) the analysis shows 98% BRE compliance (1 window out of 89) which is considered exceptional. The one window relates to a side facing window at the rear of the property. The report states that the transgression records a light change within 11.3% beyond the BRE's permissible 20% and retains an NSL value in excess of 64% room coverage. This value is


considered acceptable and an indicator for good levels of retained daylight within the room.

In terms of sunlight Annual Probable Sunlight Hours (APSH) records 97% BRE compliance. One window at 11 Empire Villas. The isolated transgression records retained annual value of 39% or greater and far exceeds the BRE's annual criterion of 25%.

In scenario 2 – the Vertical Sky Component (VSC) results record full BRE compliance (100%), commensurate with the BRE Guidelines, The NSL daylight assessment records 98% BRE compliance. The report states that the single transgression within 11 Empire Villas records a light transgression of 29.2%, 9.2% beyond the BRE's permissible 20% from former value and is considered remote and minor in nature. The sunlight assessment, the APSH, records full BRE compliance (100%).

The report concludes that “the Proposed Development will relate very well to the neighbouring residential properties and fall within the practical application of the BRE Guidelines.” It is also important to note that the Council has not got any technical evidence, nor been provided with any, which contradicts or raises doubt about the findings of the submitted report or finds fault with the methodology used.

Therefore officers remain satisfied that this report further demonstrates that the impact to the neighbouring residents will be acceptable.

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	06 July 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	5	WARD: Horley East And Salfords

APPLICATION NUMBER:	21/03303/F	VALID:	17/01/2022
APPLICANT:	F74 D Chateau S.A.R.L C/O Salmon Property Horley L	AGENT:	Lambert Smith Hampton
LOCATION:	TITAN HOUSE CROSSOAK LANE SALFORDS SURREY RH1 5EX		
DESCRIPTION:	The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

This application was deferred from the Planning Committee meeting of 8 June. The item was deferred as the motion to grant was not carried but there were no reasons for refusal put forward at the meeting.

Since that meeting the applicant has provided a sunlight and daylight report, a highway note on the no turn left condition (condition 25) and 5 amended plans showing a further reduction in height of the building.

Daylight and Sunlight Report:

The report considers two scenarios, 1 – excluding evergreen hedge along northern boundary and 2 – including evergreen hedge along northern boundary.

In scenario 1 - the Vertical Sky Component (VSC), records 91% BRE compliance and is considered very good for the area. Windows that demonstrate transgressions beyond the BRE Guidelines are considered remote and the majority record very good levels of daylight availability and No-Sky Line (NSL). In terms of sunlight Annual Probable Sunlight Hours (APSH) records 97% BRE compliance. The isolated transgression records annual sunlight levels far exceeding the BRE's 25% target value.

In scenario 2 – the Vertical Sky Component (VSC) results record full BRE compliance (100%), commensurate with the BRE Guidelines, The NSL daylight assessment records 98% BRE compliance. The single transgression is considered isolated and localised. The sunlight assessment, the APSH, records full BRE compliance (100%). Therefore officers are satisfied that this report further demonstrates that the impact to the neighbouring residents will be acceptable.

Amended plans:

Amended plans have been provided which show a further 0.5 metres reduction in the height of the buildings. The height of the parapet has been reduced from 13.7m to 13.2m and the eaves along the norther boundary have been further reduced from 12.65 to 12.15m. This has further reduced the bulk of the building along the northern boundary and as is clearly shown on the updated cross section drawings the proposal now comfortably passes both the 25 and 45 degree test. These changes further reduce any potential impact on the neighbouring properties and combined with the Daylight and Sunlight Report demonstrates that the proposed development would have an acceptable relationship with the neighbouring properties to the north of the site.

Transport note:

This report re-emphasises that the proposal would result in a reduction in vehicle traffic accessing site and travelling on the local highway network when compared to the existing use of the site. As a result of the development therefore would be 105 fewer movements in the AM peak hour, 114 fewer movements in the PM peak hour and 378 movements over the daily period. This analysis and impact was agreed with the Highway Authority. The moving of the access further along Cross Oak Lane also results in an improvement to traffic flow due to reduced likelihood of junction being blocked and potential accidents or delays. Lastly the report states that the imposition of the condition so as to not allow such vehicles to turn left out of the access would not change the peak hour traffic data analysis – because it was not recorded and not expected in the future that HGVs would come from the east down Cross Oaks Lane. From officer point of view the requirement of a condition was requested by SCC on highway safety grounds. They are the technical statutory consultees and it has been demonstrated that this condition would have no impact. Officers do note that concern was raised regarding the routing of heavy vehicles not just to the east of the site but also other narrow country lanes in the area and residential estates such as The Acres and Westvale. Officers have therefore recommended to update condition 25 to ensure that the subsequent routing strategy also ensures that other inappropriate routes are avoided.

These changes and additional information, in the view of officers, further strengthen officer's position that this scheme is entirely acceptable and there are no substantive reasons as to why this application should be refused.

Members should also note the significant economic benefits this proposal will bring to the Borough. There is a whole section in the submitted Planning Statement (Section 7) that deals with this. In short the scheme is designed to meet latent demand for E (g) (iii), B2 and B8 uses in the Coast to Capital economic area and is a response to known demand for high quality space that is well connected to


Gatwick Airport and the existing infrastructure in Gatwick Diamond. The scheme would create (blended) around 160 direct jobs during operation as well as provide local job opportunities during the demolition and construction phases. The Planning Statement also emphasises that significant weight needs to be placed on paragraph 81 of the NPPF. Paragraph 81 of the NPPF states that:

“Planning policies and decisions should help create the conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development. The approach taken should allow each area to build on its strengths, counter any weaknesses and address the challenges of the future. This is particularly important where Britain can be a global leader in driving innovation, and in areas with high levels of productivity, which should be able to capitalise on their performance and potential.”

The applicant also notes that “This is further supported given that this is an allocated site and was acquired by the applicant on the basis of the favourable planning policy position of the subject site and the desire by the Capital to Coast LEP to encourage new employment in this key growth hub around Gatwick and the wider economic triangle.”

The economic benefits of the scheme are not set out in detail within the committee report because officers found the scheme to be entirely policy compliant and within a site which is designated for such types of developments and therefore it wasn't considered necessary. However members are reminded of the need to take in to account the economic benefits identified by the applicant in their decision making.

The previous Officer report is set out below and the recommendation remains that planning permission should be granted with conditions subject to the completion of a S106. The changes to the original report set out in the addendum for the June meeting and changes to the conditions/drawings discussed above are shown in *italics* in the below report.

 Reigate & Banstead BOROUGH COUNCIL Banstead Horley Redhill Reigate	TO:	PLANNING COMMITTEE
	DATE:	08 June 2022
	REPORT OF:	HEAD OF PLANNING
	AUTHOR:	Michael Parker
	TELEPHONE:	01737 276339
	EMAIL:	Michael.parker@reigate-banstead.gov.uk
AGENDA ITEM:	7	WARD: Horley East And Salfords

APPLICATION NUMBER:	21/03303/F	VALID:	17/01/2022
APPLICANT:	F74 D Chateau S.A.R.L C/O Salmon Property Horley L	AGENT:	Lambert Smith Hampton
LOCATION:	TITAN HOUSE CROSSOAK LANE SALFORDS SURREY RH1 5EX		
DESCRIPTION:	The demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'		
All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.			

SUMMARY

The application seeks full permission for the demolition of existing buildings (2) and the erection of two any industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.

There is no in principle objection to the scheme. The site is located within the urban area, the site is not statutory or locally listed and is in a designated a Principal Employment Area.

The site is located on the eastern side of the A23 and at the southern end of the urban area of Salfords. The site was previously occupied by Titan Travel. To the east of the site is the railway line. To the north of the site are residential properties accessed along Empire Villas. The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height bridge which runs beneath the railway line. There are trees and boundary vegetation on most boundaries of the site, notably the boundaries with the A23, with the existing buildings set well back from the A23 and Cross Oak Lane and Empire Villas and this is a feature which prevails along much of this stretch of the A23

It is accepted that the scheme would result in a significant change to the existing character and nature of the site however, overall, it is considered that the proposal achieves a good standard of design and a development which is in keeping with the scale and character of surrounding industrial developments and would not have a detrimental impact on the character of the area.

Whilst neighbouring properties would experience a significant change as a result of the development, the proposals would not give rise to a serious detriment to their living conditions and thus comply with policy DES1 of the DMP and the general provisions of the NPPF (para 127) which seeks to ensure that developments provide a high standard of amenity for existing and future occupants.

Subject to conditions the proposal is considered to be acceptable with regard to highway safety and capacity, parking, trees, ecology, Gatwick safeguarding, sustainable construction, crime and flooding/drainage.

This proposal is therefore considered to be acceptable in planning terms so is recommended for approval.

RECOMMENDATION(S)

Planning permission is GRANTED subject to conditions.

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended to secure:

- (i) Contribution of £6150 towards auditing of the travel plan
- (ii) The Council's legal costs in preparing the agreement

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 8 November 2022 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason

1. Without a completed planning obligation the proposal fails to provide adequate contribution towards auditing of the Travel Plan and is therefore contrary to the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

Consultations:

Aerodrome Safeguarding (Gatwick Airport): no objection subject to conditions

Environment Agency: no objection

Environmental Health (contamination): no objection subject to conditions

Environmental Health (air quality): no objection subject to adequate dust management

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to conditions and £6150 contribution towards auditing of Travel Plan.

Horley Town Council: objects

i) The current transport infrastructure will not support the proposal both at Cross Oak Lane and at the junction of Cross Oak Lane with the A23 especially when taking into account that the Westvale Park development access road joins into this junction;

ii) Prior to any approval, a thorough traffic survey along the A23 and its junctions, from the Chequers roundabout in Horley to the junction with Three Arch Road in Earlswood, be carried out and the results established;

iii) Should the Local Planning Authority be minded to approve the application, HTC request a condition be imposed to require traffic regulation of site traffic being prevented from using access routes through the Acres and Langshott developments and country lanes in the wider Salfords and Sidlow Parish Council area.

Network Rail: no objection from planning perspective

Regulatory Support Services: no objection subject to conditions

Reigate Society: concerns regarding the impact on residential properties along Empire Villas and impact on traffic at Cross Oak Lane/Bonehurst Road junction

Salfords and Sidlow Parish Council: concerns regarding impact on residents in Empire Villas in terms of light, outlook and privacy and impact from plant/pollution and operations of the site. Concerns about the B8 use proposed in terms of type of employment provided. Concerns about impact on local road network.

Surrey County Council Minerals and Waste: no objection

Surrey County Council Lead Local Flood Authority:

Satisfied that the proposed drainage scheme meets the national guidance and technical standards. Condition recommended to secure further finalised details of drainage strategy and implementation of drainage strategy.

Surrey Wildlife Trust:

No objection subject to conditions securing mitigation measures

Thames Water: no objection in relation to waste water network and sewage treatment works infrastructure capacity. A number of informatives recommended.

Representations:

To date (publishing of 27 July agenda) 32 responses, 31 objections and 1 representation, have been received raising the following issues:

Issue	Response
Alternative location/ proposal preferred	See paragraph 6.2-6.4
Crime fears	See paragraph 6.42-6.46
Drainage/sewage capacity	See paragraph 6.39-6.41
Harm to Conservation Area	The site is not within a Conservation Area
Harm to Green Belt/countryside	The site is within the designated urban area
No need for the development	See paragraph 6.2-6.4
Harm to wildlife habitat	See paragraph 6.34-6.36
Health fears	See paragraph 6.15-6.25
Inconvenience during construction	See paragraph 6.15-6.25
Inadequate parking	See paragraph 6.26-6.30
Increase in traffic and congestion	See paragraph 6.26-6.30
Hazard to highway safety	See paragraph 6.26-6.30
Overdevelopment	See paragraph 6.5-6.14
Out of character with surrounding area	See paragraph 6.5-6.14
Poor design	See paragraph 6.5-6.14
Loss of buildings	See paragraph 6.2-6.4
Noise & disturbance	See paragraph 6.15-6.25
Loss of/harm to trees	See paragraph 6.31-6.33
Overlooking and loss of privacy	See paragraph 6.15-6.25
Loss of private view	Not a material planning consideration
Overshadowing	See paragraph 6.15-6.25
Overbearing relationship	See paragraph 6.15-6.25
Concerns about subsidence damage from construction	This is not a material planning consideration but a matter

	between the applicant and neighbouring properties. The proposal would require building regulations approval
Property devalue	Not a material planning consideration
Concern about the retention of the Philips Research Laboratories Plaque	See paragraph 6.51

1.0 Site and Character Appraisal

- 1.1 The site is located on the eastern side of the A23 and at the southern end of the urban area of Salfords. The site was previously occupied by Titan Travel. To the east of the site is the railway line. To the north of the site are residential properties accessed along Empire Villas. The site accesses onto Cross Oak Lane, close to the signalised junction with the A23. To the east, is a restricted height bridge which runs beneath the railway line. There are trees and boundary vegetation on most boundaries of the site, notably the boundaries with the A23, with the existing buildings set well back from the A23 and Cross Oak Lane and Empire Villas and this is a feature which prevails along much of this stretch of the A23.
- 1.2 The site is designated as a Principal Employment Area – Salfords, it is also within the designated urban area and the site adjoins Flood Zone 2 and 3 to the south of the site.

2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: A pre-application submission was made to the Council regarding this development (PAM/21/00190). This has given the Council the opportunity to set out the key areas of consideration and areas where further information would be required and also to highlight areas of concerns.
- 2.2 Improvements secured during the course of the application: Additional contamination information, tree reports, ecology information, additional information regarding highway/parking concerns and amended plans lowering parapet on northern elevation. Details of location of temporary school also provided.
- 2.3 Further improvements to be secured through conditions or legal agreement: Further details of Materials, CTMP, CMS, Travel Plan, Ecology, Trees and landscaping, noise and use, sustainable construction.

3.0 Relevant Planning and Enforcement History

- 3.1 07/01810/OUT Outline planning application for the phased redevelopment of the site to form an office Granted

campus for Titan Travel comprising 2 x 2 storey, 2 x 3 storey and 1 x 4 storey building, associated parking and landscaping. Additional/amended plans received on 13/11/2007 showing parking and landscaping. (Drwg No. 210 Rev C, 203 (Additional Plan), SUDS Assessment (received 13.11.07)) Additional/Amended plans received on 15/04/2008 to show Site plans, elevations, phasing and demolition plans (Drwg no (Amended plans 884 200C, 884 201C, 884 202, 884 203B, 884 210G, 884 211C, 884 212D, 884 220D, 884 222C, 884 230D, 884 232D) (Additional plans MV01, MV02, MV03, MV04)) Amended plans received on 12/05/2008 to show Amended site layout plan (Drwg no 210 H).

- | | | | |
|-----|--------------|---|--------------|
| 3.2 | 22/00486/DED | Demolition of titan house, gate house, electricity house. As amended on 16/03/2022, 23/03/2022 and on 06/04/2022. | No objection |
|-----|--------------|---|--------------|

Site to south of Cross Oak Lane - Goya Developments Former Philips Research Laboratories South Site Crossoak Lane Salfords Surrey RH1 5HA:

- | | | | |
|-----|--------------|--|---------|
| 3.3 | 18/01180/F | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. As amended on 30/07/2018, 15/08/2018 and on 04/10/2018. As amended on 19/10/2018 and on 23/10/2018. | Granted |
| 3.4 | 19/01370/S73 | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. - Application to remove condition 23 of permission 18/01180/F which restricts the amount of B8 floorspace. As amended on 06/09/2019 | Granted |
| 3.5 | 19/02199/S73 | The redevelopment of the site to include four employment buildings incorporating 5 units for open B1(b), B1(c), B2 and B8 use comprising 15,831sqm GEA with associated parking and landscape planting. - Application to remove condition 23 of | Granted |

permission 18/01180/F which restricts the amount of B8 floorspace. Variation of conditions 1, 7, 11 and 15 of permission 19/01370/S73. Amendment to approved plans.

4.0 Proposal and design approach

- 4.1 This is a full application seeking permission for the demolition of existing buildings and the erection of two industrial processes (class e (g) (iii)), general industrial (use class b2) storage and/or distribution (use class b8) units with ancillary office accommodation, together with other associated parking, servicing landscape and infrastructure.'
- 4.2 The proposed development comprises
- 8,152.3 sq.m (GEA) across two (2) industrial process, general industrial and/or storage and distribution units, these units (A and B) would be positioned at the northern end of the site and would spread across the majority of the width of the site;
 - The proposal includes 892 sq.m (GIA) ancillary office in mezzanines (12% of space) across the two units and contained in two storeys;
 - Unit A will provide two loading bays with 3 dock levellers, Unit B will provide 2 loading bays
 - Core parking at 88 staff and visitor car parking spaces located to the south of units A and B, with 28 additional flexible shared parking) taking total to 119 spaces if required;
 - 5 (or 6%) accessible car parking spaces;
 - 5% (or 4 double posts, 2 per unit) electric vehicle charging points (EVCP) with the car parking areas with the remainder of the staff car parking prepared with duct work for future EVCP upgrade;
 - Policy compliant secure cycle spaces (20 spaces);
 - 11 individual trees are to be removed (with 1 small tree in the groups) of 42 existing individual trees (4 are advised to be removed regardless of development – Category U), and replaced with 37 native trees and native ornamental (21 extra heavy standard, 5 conifer/pines and 6 x 1+1 transplants) that will be replanted in the new remodelling landscaping areas (this replaces 11+1 trees to be removed) this represent above a 3:1 ratio and results in an increase level of native trees on site;
 - The proposed tree planting will result in a 60% increase in overall tree coverage from 42 mixed species trees to 67 native and native ornamental trees;
 - Delivering minimum BREEAM Very Good rated buildings;
 - Retention and reposition of the 'Phillips Laboratories' Blue Plaque celebrating the world leading research and development that took place at the site post war; and
 - Photovoltaic solar panels are proposed on the roofs of the two units and the level of provision will exceed Part L of the Building Regulations and the Energy Strategy.

- 4.3 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:
Assessment;
Involvement;
Evaluation; and
Design.
- 4.4 Evidence of the applicant’s design approach is set out below:

Assessment	Section 2.0, 2.1 and 3.0 set out the context of the location, existing site and setting of the site.
Involvement	The design and access statement is silent on this matter but the Planning Statement at para. 6.63 and 6.64 and submitted Statement of Community Involvement (December 2021) outline the consultation process with identified stakeholders and the outcome of the process.
Evaluation	The proposed design has been led by the needs of the applicant, pre-application discussion, advice from the tree officer and stakeholders such as commercial property agents
Design	<p>Section 4.0 to 10.0 set out the design proposals. The site layout has been developed to respond to the site constraints and the practical considerations of a development of this nature. The sizes and arrangements of the units have been developed with the design team and commercial property agents to meet local demands and prospective tenant’s requirements.</p> <p>Great care has been taken in positioning the building to best screen the residential properties to the north from the site activities and light spillage. The building frontages face onto Cross Oak Lane making them clearly visible for the access road. The office accommodation provides feature on the frontage and, as they face south, allows for good daylight and sunlight to the office areas.</p> <p>The elevations submitted with this application indicate general appearance of the proposed building. The forms are simple and well-proportioned suited to buildings of this nature, with clear internal volumes to the warehouses and three storey offices to the front keeping the warehouses free of any intrusions.</p>

- 4.5 Further details of the development are as follows:

Site area	1.64 hectares
Existing use	Office and general industry buildings
Proposed use	Class E (g) iii, B2 B8
Existing parking spaces	168 cars spaces, 68 Light goods vehicles / public carrier vehicles
Proposed parking spaces	88 car parking space (with potential for additional 31 spaces – total 119), 10 HGV loading/parking spaces.
Existing floor area	4786 m2
Proposed floor area	7805 m2 (net gain 3142m2)

5.0 Policy Context

5.1 Designation

Urban Area
Principal Employment Area - Salfords
Flood Zone 2 and 3 - to south of site

5.2 Reigate and Banstead Core Strategy

CS1 (Sustainable Development)
CS2 (Valued landscapes and the natural environment)
CS5 (Valued people and economic development)
CS10 (Sustainable Development)
CS11 (Sustainable Construction)
CS12 (Infrastructure Delivery)
CS17 (Travel options and accessibility)

5.3 Reigate and Banstead Local Plan: Development Management Plan

EMP4 (safeguarding employment land and premises),
EMP5 Local Skills and training opportunities),
DES1 (Design of new development),
DES8 (Construction Management),
DES9 (Pollution and contaminated land),
TAP1 (Access, parking and servicing),
CCF1 (Climate change mitigation),
CCF2 (Flood Risk),
NHE2 (Protecting and enhancing biodiversity and areas of geological importance),
NHE3 (Protecting trees, woodland areas and natural habitats),
INF3 (Electronic communication networks),
EMP4 (safeguarding employment land and premises),
EMP5 Local Skills and training opportunities),
DES1 (Design of new development),

DES8 (Construction Management),
DES9 (Pollution and contaminated land),
TAP1 (Access, parking and servicing),
CCF1 (Climate change mitigation),
CCF2 (Flood Risk),
NHE2 (Protecting and enhancing biodiversity and areas of geological importance),
NHE3 (Protecting trees, woodland areas and natural habitats),
INF3 (Electronic communication networks)

5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance

A Parking Strategy for Surrey
Parking Standards for Development
Local Character and Distinctiveness
Design Guide SPD
Climate Change and Sustainable
Construction SPD

Other

Human Rights Act 1998
Community Infrastructure Levy
Regulations 2010
The List of Historic Parks and
Gardens

6.0 **Assessment**

6.1 The main issues to consider are:

- Principle of development
- Design and impact on character of the area
- Neighbour amenity
- Access, parking and traffic generation
- Trees
- Ecology
- Sustainable construction
- Flooding and Drainage matters
- Crime
- Community Infrastructure Levy (CIL) and requested contributions
- Other matters

Principle of development

6.2 The site is located within the Salfords Principal Employment Area. Policy EMP1 states that "Planning permission will be granted for change of use to

offices, industrial, and storage and distribution, and for the development of new, upgraded or extended floor space within these uses.”

- 6.3 The existing site and buildings is not statutory or locally listed and therefore there is no objection to its demolition, which has already been allowed under a prior approval application.
- 6.4 There is an identified need for all employment uses within the available evidence and therefore the proposal to re-develop the site for employment uses is acceptable in principle.

Design and impact on character of the area

- 6.5 It is acknowledged that the proposed redevelopment of the site is significant in scale. Units A and B would measure 13.7m to top of parapet and approximately 15.2m at highest point (in centre of roof). Together unit A and B would spread across almost the full width of the site at approximately 127.5. remainder of the site would be made up of hardstanding and landscaping, including the retention of the tree buffer along the western boundary with the main road. The proposed development would therefore result in a significant change to the quantum of development on the site.
- 6.6 However, the proposal needs to be assessed in the context of the fact that the site is designated as a Principal Employment Area and its brownfield status where it is important to make efficient use of the site. The character and land uses in the surrounding area which are of industrial and commercial developments, particularly the recently developed site to the south of the site and the nature of the proposed use, which necessitates buildings of a certain scale and design.
- 6.7 In terms of the layout and scale of the buildings this is considered typical for a modern industrial/commercial development with large buildings measuring up to 15m tall and a significant area of hardstanding for both HGV and car parking. The density study submitted by the applicant shows the proposal site in the context of the wider Salfords Industrial Estate to the north and south of the site, as well as comparison with the recently development site (Goya site) immediately to the south. The Density study clearly shows that the scale and density of the proposal is in line with the general pattern of development within the Industrial Estate. In terms of the comparison with the Goya site the proposed density would be 48.2% compared to 48.8%, the proposed site coverage would be 44% compared to 43% and the proposed heights (clear internal height) would be in keeping with the site to south which ranges from 10.5m to 12.5m. It is therefore considered that considered in this context the proposal would not be out of keeping with the general character of the wider industrial estate.
- 6.8 The proposed design of the units would be fairly simple and utilitarian but this is the nature of such buildings and given the location of the site within a designated industrial estate it is considered that the proposed buildings could not be refused on this ground. Officers have asked the applicant to consider

ways in which they could add interest and break up the elevations. The applicant has indicated that they are willing to review the finish of the elevations in terms of cladding and colours proposed. It is considered that were the application approved a condition could be added to secure further details.

- 6.9 Another important factor on this site is the existing treed nature of the western boundary and south-western corner of the site. The impact on trees is discussed in more detail below but in terms of the location of the building the closest part of the proposal would be approximately 15.3m from Bonehurst Road, which is commensurate with the spacing left by the development to the south (approx. 15.5m). This spacing ensures that the trees along the western boundary can be retained and provides an adequate set back from the road to limit the visual impact of the proposal on the streetscene.
- 6.10 In terms of the potential impact on the wider landscape the application is accompanied with a Strategic Landscape and Visual Impact Assessment (SLVIA). Regarding potential landscape impacts, the SLVIA confirms that there are no known registered or designated heritage assets on the site, and that its current landscape quality and condition is classified as 'Poor', with no protective landscape designations. Accordingly, the SLVIA concludes that the proposed site design would result in a negligible to slight beneficial effect on the landscape features and character and a negligible effect on the land value, hydrology, cultural heritage, land use and topography.
- 6.11 The SLVIA also considers the visual effects of the proposals by identifying viewpoints and the potential impact on the surrounding residential properties, motorists and surrounding industrial development, referred to as visual receptors. In summary, the SLVIA concludes that, the proposals will result in negligible impacts on the residential and employment development and motorists in the vicinity of the site.
- 6.12 Overall, the SLVIA concludes that, through the management and enhancement of the existing vegetation will help screen the development and integrate it within the surrounding landscape to the east and west, whilst improving the landscape value to the north and south.
- 6.13 Therefore, whilst the scheme would result in a significant change to the existing character and nature of the site it is considered that the proposal achieves a good standard of design and a development which is in keeping with the scale and character of surrounding industrial developments and would not have a detrimental impact on the character of the area.
- 6.14 Conditions are recommended to secure finalised levels details, and further details of the proposed external materials, boundary treatments and hard landscaping.

Neighbour amenity

- 6.15 Development Management Policy DES1 expects all new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way or overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.
- 6.16 The site borders a road to the west, the railway to the east and a road with industrial/commercial units to the south. As such there would be no concerns with the relationship to these neighbouring land uses.
- 6.17 To the north is a residential road known as Empire Villas which has the potential to be impacted by the proposal in terms of overbearing impact, loss of light and loss of privacy. In terms of privacy there would be no north facing windows to the building ensuring no opportunity for overlooking.
- 6.18 In terms of overbearing impact and loss of light to the nearest properties no. 1 to 8 Empire Villas would be located a minimum of approximately 25m from the closest part of the building and these properties front on to Empire Villas so the rear of their properties would be unaffected in terms of outlook. The applicant has demonstrated that the new building would pass the 25 degree test which indicates that the impact on light would be acceptable. Given this and the distance between these dwellings and the proposed building and the retained screening it is not considered that the proposal would result in an unacceptable loss of light or overbearing impact.
- 6.19 No.11 Empire Villas, located at the eastern end of Empire Villas would be the nearest dwelling, at approximately 11.9m from the proposed building. The house is however orientated east-west and therefore the main outlook for this property is not towards the building. When a 45 degree line is made from the top of the proposed building the line would not conflict with any of the nearest east or west facing windows indicating that there would not be an unacceptable loss of light. There is a side window facing the proposed building which would fail the 25 degree test however from a site visit it was clear that this window was a secondary window and therefore the room it serves would not be unacceptably impacted. Whilst the proposed building would be significant in height it is considered that the distance of over 10metres to the side of no.11 combined with the east – west orientation would ensure that there is not an unacceptable loss of outlook or overbearing impact.
- 6.20 No.28 Beechwood Villas would have a similar relationship to no.11 but the spacing would be greater. As such for similar reasons as no.11 the impact is considered to be acceptable.
- 6.21 In terms of the possible impact of the proposal on the dwellings to north from matters such as noise and light spill the Council has consulted its Environmental Health consultants Regulatory Support Services (RSS). They have reviewed the proposed plans and submitted technical reports and has

raised no objection subject to conditions to limits impact on the residential properties to the north in terms of noise and lighting. One of the recommended conditions relates to Construction and Environmental Management Plan. These requirements will be added in to the Construction Management Statement (CMS) as discussed further below.

- 6.22 In terms of contamination the applicant has submitted a Phase 1 and Phase 2 report. The contamination officer has reviewed the information and has advised that “For the sites redevelopment application, the contaminated land conditions would still apply as further Phase 2 intrusive investigations reporting would need to be submitted in relation to that application”. As such conditions are recommended to secure further information.
- 6.23 With regard to Air Quality the Council’s Environmental Health Officer has raised no concern having reviewed the submitted Air Quality Assessment in terms of the proposed use of the site. They have asked that a suitable dust management scheme during construction is secured by condition. This will be covered within the CMS as set out below.
- 6.24 In terms of inconvenience during the construction period due to the proximity of the site to the residential dwellings to the north of the site it is accepted that there is potential for noise and disruption to these properties. It is not a reason to refuse an application given the temporary nature of the construction but it is considered that the potential impact could be minimised through the inclusion of a Construction Management Statement (CMS) which can secure further information in relation to matters such as working hours and procedures in place to reduce dust and noise emissions. The CMS can be secured by condition. As set out in the below transport section a condition is also recommended to secure further details of construction traffic, parking and storage management through a Construction Transport Management Plan (CTMP).
- 6.25 Taking the above into account, whilst neighbouring properties would experience a significant change as a result of the development, the proposals would not give rise to a serious detriment to their living conditions and thus comply with policy DES1 of the DMP and the general provisions of the NPPF (para 127) which seeks to ensure that developments provide a high standard of amenity for existing and future occupants.

Access, parking and traffic generation

- 6.26 Development Management Plan Policy TAP1 requires all types of development to provide safe and convenient access for all road users taking account of cumulative impacts, which would not unnecessarily impede the free flow of traffic, or compromise pedestrians or other transport modes. Traffic resulting from a development must not materially impede traffic congestion on the highway network or increase the risk of accidents.
- 6.27 The National Planning Policy Framework at paragraph 109 confirms that “Development should only be prevented or refused on highways grounds if

there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.”

6.28 There would be new access proposed as part of the development and the parking provision would change from 168 cars spaces, 68 Light goods vehicles / public carrier vehicles to 88 car parking space (with potential for additional 31 spaces – total 119), and 10 HGV loading/parking spaces, reflecting the lower on site employment generating potential of the proposed use.

6.29 Surrey County Council as the County Highway Authority (CHA) has considered this additional information and has provided the following comments:

“The proposed development would lead to a reduction in existing two way movements from 136 movements in the am peak and 142 movements in the pm peak and 752 during the day to 31 two way movements in the am peak and 28 two way movement in the pm peak and 374 movements during the day. As such there has been no junction assessment carried out due to the reduction in movements.

The reduction is due to the proposed change of use from a predominantly office based use with some workshops to a Use Class E (g) (iii) and B2 and B8. It is recommend that a condition is imposed that the site does not become an office based development as this has not been proposed and therefore assessed.

The proposed quantum of parking for any use applied for is adequate and comparable to what historically has occurred on the site. In addition there are parking restrictions on Cross Oak Lane between its junction with Bonehurst Road and the rail bridge to the east of the access. Those parking restrictions would prevent inappropriate parking on the highway.

The layout of the site would be able to accommodate large vehicles likely to use the site, such vehicles would be able to enter and leave the site in forward gear.

The proposed access has been subject to a road safety audit, which recommended some modifications to the purposed access that can be incorporated into the access at the detailed design stage for subsequent Section 278 agreement works to build the access. One such concern at the safety audit is tall vehicles colliding with the bridge to the east. I have recommended a condition for the developer to submit a routing strategy to be submitted to and agreed in writing with the Local Planning Authority.”

6.30 In light of the above comments from the CHA the application is considered to have an acceptable highways and parking impact and is therefore considered to be compliant with policy TAP1.

Impact on trees

- 6.31 The applicant has included an Arboricultural Impact Assessment and Landscape Plan. The Tree Officer provided the following initial comments:
“The Arboricultural Impact Assessment from Landmark Trees is well detailed and thorough and explains how the majority of site trees can be retained during the proposed development and explains the removal of those trees that will need to be taken out.

The Tree Protection Plan demonstrates the protection of the retained trees. The line of Tree Protection Fencing seems to stop though before the eastern end of the line along the north side boundary and misses off the eastern part of G3 – this must be either rectified or justified before the plan can be approved.

The Tree Protection Plan also shows parts of the root protection area of some retained trees that are outside of the protective fence line. These areas are shown to be protected by ‘Ground Protection’ with the finished surface explained as of a ‘no-dig’ construction in the AIA. This is briefly and generally explained in the AIA and it looks to be proposed that the ‘no-dig’ base will be laid down as initial ground protection with the surface renewed when construction is completed. This sounds like a useful proposal, but it is not clear how this will work as part of the whole site – for example there may be level changes – also, the demolition and construction machinery and the post-development site traffic is likely to be heavy. It is suggested that the details on this could be provided via a conditioned AMS – which is so, however, if this detail is critical to the proposal, then it would be better to have a final specification up front with validation from an engineer and arboriculturist to ensure that it will all work and fit together on the ground.

The proposed landscape plan is detailed and extensive and should provide landscape screening and habitat value to the site. There is a concern that the new planting along the northwest and north side boundaries may struggle to establish in such proximity to the proposed building.”

- 6.32 Following the submission of additional information the Tree Officer has provided the following comments:

“In respect to earlier comments the ground protection position and function is now clear. However, it is what comes next that doesn’t yet seem to be finalized. The AIA document says at 8.2.5 that ‘Where sections of hard surfacing are required in close proximity to trees, it is recommended that ‘No Dig’ surfacing be employed in accordance with BS5837...and AAIS 1996[APN1]’. It also says at 8.2.6 that ‘If the RPA of a tree is encroached by underground service routes then BS5837...and NJUG Volume 4 provisions should be employed. If it is deemed necessary, further arboricultural advice must be sought’.

These are commonplace issue with trees and development, and it looks as though the ‘no-dig’ surfacing could be quite extensive given the amount of ground protection over areas of RPA not within the protective fencing. The

above are not Method Statements though and it is therefore not clear how this will work on site. The above points are understood – it is known what it means – but it is general and therefore it is not certain what will be done. It is common for the specifics of these matters to be designed post consent; however, these operations can be the most disruptive and can lead to irreparable damage to the roots systems of retained trees and their rooting environment if not properly carried out, and sometimes it just doesn't work.

As per my previous comments: 'It is suggested that the details on this could be provided via a conditioned AMS – which is so, however, if this detail is critical to the proposal, then it would be better to have a final specification up front with validation from an engineer and arboriculturist to ensure that it will all work and fit together on the ground'."

- 6.33 In light of the above comments from the Tree Officer and subject to conditions the application is considered to have an acceptable impact on the existing site and surrounding trees and the proposal is therefore compliant with policy NHE3 of the Development Management Plan 2019.

Ecology

- 6.34 The planning application is supported by a Preliminary Ecological Appraisal (PEA). The report concludes that the site has relatively low ecological significance with no impact on statutory or non-statutory site and unlikely to be any impact on protected species. Surrey Wildlife Trust (SWT) has considered the submitted report and following further information regarding bats has advised that the reports and surveys have been produced in line with best practice and therefore subject to a condition securing a Construction Environmental Management Plan (CEMP) and Landscape Ecological Management Plan (LEMP) they consider that the impact would be acceptable.
- 6.35 In terms of net gain in biodiversity the submitted documents show that the scheme will not provide a net gain. In such circumstances policy NHE2(b) does allow for a contribution towards off site provision to off-set this impact. The applicant has made clear that they are willing to agree to such a contribution however the Council currently has no mechanism to allow for an off-site contribution, with no projects or sites currently identified for this. It is noted that the NPPF (para 180 d) requires that when determining planning application Local Planning Authorities should apply the following principle "development whose primary objective is to conserve or enhance biodiversity should be supported; while opportunities to improve biodiversity in and around developments should be integrated as part of their design, especially where this can secure measurable net gains for biodiversity or enhance public access to nature where this is appropriate." However the NPPF does not require a measurable net gain and policy NHE2 5b. states that schemes will be expected to "be designed, wherever possible, to achieve a net gain in biodiversity." In this case the applicant has set out that it is not possible to achieve a net gain in biodiversity and given the national and local policy position it is not considered that this could form a reasonable reason for

refusal. The proposal does however include a number of on-site enhancement measures and the LEMP condition recommend by SWT would secure further details of these measures as well as future maintenance.

- 6.36 Subject to the conditions discussed it is considered that the scheme would comply with policy NHE2 of the DMP.

Sustainable Construction

- 6.37 The proposal is required to meet policy CS11 of the Core Strategy and policy CCF1 of Development Management Plan. This requires the development to meet BREEAM 'very good' and to include renewable or low-carbon energy generation to provide 10% of the expected energy usage of the development.
- 6.38 The submitted reports confirm that the building is aiming to meet the BREEAM 'very good' rating and that it would be able to achieve a 19.9% reduction in carbon emissions through PV panels. Conditions are recommended to secure implementation.

Flooding and Drainage matters

- 6.39 The site is in Flood Zone 1 and is therefore at a low risk of fluvial flooding but the road and land to the south of the site is within flood zone 1 and 2. The site is designated at being at risk of surface water flooding. A Flood Risk Assessment and Drainage strategy document has been submitted with the application.
- 6.40 The Flood Risk Assessment has been assessed by the Environment Agency who has raised no objection to the proposals and has not recommended any conditions.
- 6.41 The drainage strategy has been considered by Surrey County Council as the Lead Local Flood Authority who has concluded that it meets the requirements of national technical standards. They therefore raise no objection subject to a condition securing finalised details of the drainage strategy and implementation.

Crime

- 6.42 Policy DES1 requires that development: "Creates a safe environment, incorporating measures to reduce opportunities for crime and maximising opportunities for natural surveillance of public places. Developments should incorporate measures and principles recommended by Secured by Design."
- 6.43 The submitted Design and Access Statement has a section on security which states that the following have been designed into the scheme:
- Access and movement has been well considered to create well-defined routes and entrances that provide convenient movement throughout the development, without compromising security.
 - The layout has been developed so different uses do not cause conflict.

- All publicly accessible spaces have been orientated in a way that they are all overlooked.
- A sense of safety will be achieved through the level of human activity that will be present throughout the day.

- 6.44 The development has only one access point into the site, thus providing a secure entrance point and visibility for each of the units, offering natural surveillance for the proposed building units.
- 6.45 The sides and rear of the site, as well as the yard areas will be enclosed by the use of 2.4m high secure weld mesh fencing, which will help mitigate the risk of crime, providing safety for employers and employees, thus preventing the intrusion of trespassers.
- 6.46 In order to secure further information a condition in relation to Secure by Design is recommended.

Community Infrastructure Levy (CIL) and requested contributions

- 6.47 The proposal, being for new industrial and distribution premises, falls outside of the uses which attract a charge based on the Council's adopted Charging Schedule and as such the development would not be liable to pay CIL.
- 6.48 The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 and state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence including costed spending plans to demonstrate what the money requested would be spent on.
- 6.49 In this case, as above, the County Council has requested contributions towards travel plan monitoring (£6,150). These contributions are considered necessary to address and avoid potential unacceptable highways impacts and are proportionate in their scale and kind to the development proposed. A clear justification has been provided by the County Highway Authority. These will be secured through a legal agreement.

Other matters

- 6.50 The site is close to Gatwick airport and therefore it has been examined by Gatwick Airport from an aerodrome safeguarding perspective. No objection has been raised subject to a condition to secure a Bird Hazard Management Plan and details of the proposed solar panels.
- 6.51 Representations have been received asking that the existing commemorative plaque regarding the historic use of the site for the Philips Research Laboratory. The applicant has confirmed they are happy to protect the

plaque during construction and retain it on site. A condition is recommended to secure this.

CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
<i>Roof Plan</i>	2105-PL-1002	P2	21.04.2022
<i>Roof Plan</i>	2105-PL-1006	P2	21.04.2022
<i>Elevation Plan</i>	2105-PL-1015	P4	24.06.2022
<i>Elevation Plan</i>	2105-PL-1016	P4	24.06.2022
<i>Section Plan</i>	2105-PL-0020	P3	21.06.2022
<i>Elevation Plan</i>	2105-PL-0021	P4	24.06.2022
<i>Section Plan</i>	2105-PL-1020	P3	21.06.2022
Site Layout Plan	2105-PL-0001	P3	06.05.2022
Site Layout Plan	2105-PL-0003	P3	06.05.2022
<i>Location Plan</i>	2105-EX-0099	P1	06.06.2022
Landscaping Plan	21-120-P-01	A	10.01.2022
Existing Plans	2105-EX-0021	P1	29.12.2021
Existing Plans	2105-EX-0001	P1	29.12.2021
Existing Plans	2105-EX-0020	P1	29.12.2021
Other Plan	2105-PL-0002	P1	29.12.2021
<i>Floor Plan</i>	2105-PL-1000	P1	29.12.2021
Floor Plan	2105-PL-1001	P1	29.12.2021
Floor Plan	2105-PL-1005	P1	29.12.2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall commence, excluding demolition and enabling works, until a Construction Management Statement (CMS), to include details of:
 - a) An introduction consisting of construction phase environmental management plan, definitions and abbreviations and project description and location;
 - b) A description of management responsibilities;
 - c) A description of the construction programme which identifies activities likely to cause high levels of noise or dust;
 - d) Prediction of potential impacts with regard to water, waste, vibration, emissions and odours. Where potential impacts are identified, mitigation measures should be identified to address these impacts.

- e) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
- f) Means of communication and liaison with LPA, neighbouring residents and businesses.
- g) Site working hours and a named person for residents to contact;
- h) Detailed Site logistics arrangements;
- i) Details regarding parking, deliveries, and storage;
- j) Details regarding dust and noise mitigation measures to be deployed including identification of sensitive receptors and ongoing monitoring;
- k) Details of the hours of works and other measures to mitigate the impact of construction on the amenity of the area

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

4. No development shall take place, excluding demolition and enabling works, until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining buildings and to safeguard the visual amenities of the locality including heritage assets with regard to Reigate and Banstead Development Management Plan DES1 and NHE9.

5. Prior to commencement of development, excluding demolition and enabling works, in follow-up to the contamination information already supplied, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local

Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

6. Prior to commencement of the development, excluding demolition and enabling works, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed in line with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

7. a. Prior to commencement of the development, excluding demolition and enabling works, a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.
 - b. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings.

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

8. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

9. Prior to the commencement of any development works, including demolition and all construction activities, all tree protection measures shall be undertaken in strict accordance with the approved details contained in the submitted Arboricultural Impact Assessment Report ref. WRA/COL/AIA/01e and Arboricultural Method Statement – Demolition Stage ref. WRA/COL/AMS/01 from Landmark Trees. All arboricultural matters will then follow that described in these approved details.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

10. No development shall commence, excluding demolition works, until a detailed, scaled and finalized Tree Protection Plan (TPP) and the related Arboricultural Method Statement for the Construction Stage (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas (RPAs) of trees, shown to scale on the TPP. This must include details of all service routes, materials and methodology for any excavation and construction within the RPA of retained trees and a schedule of arboricultural supervision and reporting. All works must be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance

of the area and to comply with policies NHE3, DES1 and DES3 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

11. The development hereby permitted shall not commence, excluding demolition and enabling works, until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:
- a) The results of further infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels.
 - b) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 8.3l/s.
 - c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.). Confirmation is required of a 1m unsaturated zone from the base of any proposed soakaway to the seasonal high groundwater level and confirmation of half-drain times.
 - d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.
 - e) Details of drainage management responsibilities and maintenance regimes for the drainage system.
 - f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

12. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the surface water drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls), and confirm any defects have been rectified.

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

13. No development shall commence on site, excluding demolition and enabling works, until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), including details of existing landscape features to be retained or pruned, has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

14. No pruning, removal or other works to the retained trees and hedges located both within and overhanging the site, shall take place during construction, or for 5 years after completion except with the prior written approval of the Local Planning Authority. Any tree works already approved as part of this consent and any other work undertaken should be done in accordance with British Standard BS 3998:2010 'Tree Work - Recommendations'. If any of the retained trees or hedges, within the site, controlled by this condition, are removed, die, or become damaged or diseased within five years of completion, they shall be replaced before the expiry of one calendar year by tree/s or hedge/s to a planting specification agreed in writing by the Local Planning Authority.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard BS 5837:2012 'Trees in relation to design, demolition and construction – Recommendations'

15. The development hereby permitted shall not commence, excluding demolition and enabling works, until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:

- Management of any flat/shallow pitched roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds.

The Bird Hazard Management plan shall be implemented as approved upon completion of the roofs and shall remain in force for the life of the buildings. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: To manage the roofs in order to minimise their attractiveness to birds which could endanger the safe movement of aircraft and the operation of Gatwick Airport.

16. No solar panels are to be installed until full details of the proposed PV scheme has been submitted to and approved in writing by the Local Planning Authority.

The scheme shall be implemented as approved and no subsequent alterations to the approved scheme are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: to ensure the development does not endanger the safe movement of aircraft or the operation of Gatwick Airport through interference with communication, navigational aids and surveillance equipment and glare issues.

17. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof and the type and colour of walls and any cladding, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

18. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The

boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: In the interest of maintaining the historic and architectural character of the listed building, historic gardens and the visual amenities of the area with regard to Reigate and Banstead Development Management Plan 2019 policies DES1.

19. The development hereby approved shall not be commenced, excluding demolition and enabling works, unless and until a scheme for the protection and retention/reinstatement of the existing commemorative Philips Research Laboratory plaque has been submitted to and approved in writing by the Local Planning Authority. This shall include any necessary report works and details of its proposed location within the site.

Reason: In order to secure a realistic strategy for the preservation of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and the provisions of the NPPF.

20. No development shall commence, excluding demolition and enabling works, until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) programme of works (including measures for traffic management)
 - (e) provision of boundary hoarding behind any visibility zones
 - (g) vehicle routing
 - (h) measures to prevent the deposit of materials on the highway
 - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
 - (k) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

21. The development shall not be occupied until the proposed belmouth vehicular access to Cross Oak Lane has been constructed in accordance with the approved plan numbered TPHS/276/DR/12.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Policy TAP1 Parking, access , and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

22. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved plan numbered 2105 PL 001 Rev3 *and/or* 2105 PL 0003 Rev 3 (*depending on use of development*) for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

23. The development hereby approved shall not be occupied unless and until each of the proposed parking spaces are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) and an electrical supply for future fast charge sockets in accordance with the approved plans numbered 2105 PL 001 Rev3 *and/or* 2105 PL 0003 Rev 3 (*depending on use of development*) and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

24. Notwithstanding the submitted April 2022 Framework Travel Plan (Draft) prior to the occupation of the development a revised Travel Plan shall be submitted taking account of the occupants of the site for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide". And then the approved Travel Plan shall be implemented upon occupation of the site and for each and every subsequent occupation of the development, thereafter maintain and develop the Travel Plan to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

25. *The proposed development shall not be occupied until a routing strategy to prevent tall/heavy vehicles heading east from the access and to access the site from the east and to avoid other narrow country road and residential*

estates (such as The Acres and Westvale) in the surrounding area has been submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

26. The development hereby approved shall not be first occupied unless and until the following facilities have been provided in accordance with a scheme to be submitted to and approved in writing for:

(a) The secure parking for bicycles within the development site in line with the minimum number required by the Development Management Plan Annex 4.

And thereafter the said approved facilities shall be provided, retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2021 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

27. No development shall commence, *excluding demolition and enabling works*, until a Construction Environment Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The CEMP shall include, but not be limited to:

- a) Map showing the location of all of the ecological features
- b) Risk assessment of the potentially damaging construction activities
- c) Practical measures to avoid and reduce impacts during construction
- d) Location and timing of works to avoid harm to biodiversity features
- e) Responsible persons and lines of communication
- f) Use of protected fences, exclusion barriers and warning signs.
- g) Tree Removal and Bats Strategy and Method Statement

The development shall only be carried out in accordance with the agreed mitigation measures.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

28. No development shall commence, excluding demolition, enabling works and above ground works, until a Landscape and Ecological Management Plan (LEMP) has been submitted to and approved in writing by the Local Planning Authority (LPA). The LEMP should be based on the proposed impact avoidance, mitigation and enhancement measures specified in the submitted ecology reports and shall include, but not be limited to following:

- a) Description and evaluation of features to be managed
- b) Ecological trends and constraints on site that might influence management
- c) Aims and objectives of management
- d) Appropriate management options for achieving aims and objectives
- e) Prescriptions for management actions, together with a plan of management compartments
- f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a five-year period
- g) Details of the body or organisation responsible for implementation of the plan
- h) Ongoing monitoring and remedial measures
- i) Legal and funding mechanisms by which the long-term implementation of the plan will be secured by the applicant with the management body(ies) responsible for its delivery.
- j) Monitoring strategy, including details of how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme.
- k) Sensitive lighting plan

The agreed details shall be implemented before occupation of this development, unless otherwise agreed in writing by the LPA, and maintained/monitored in accordance with the agreed details.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2.

29. The development hereby approved shall not be first occupied unless and until the renewable energy measures detailed in the submitted Energy Assessment Report by Stroma ref 08-21-88654ES1, dated 13/12/2021, have been implemented, installed and made operational to ensure that the proposal includes renewable or low-carbon energy generation to provide 10% of the expected energy usage of the development.

Any change to the Energy Strategy and timing of implementation shall only be made once an updated report has been submitted to and agreed in writing by the LPA.

Thereafter the scheme development shall be maintained in accordance with the agreed details.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS11 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

30. The development hereby approved shall not be first occupied unless and until evidence has been provided that the development has met a minimum of BREEAM 'very good'.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS11 of the Reigate & Banstead Core Strategy 2014.

31. Full details of the measure taken to implement the recommendations detailed in the Noise Assessment report prepared by Stroma reference SBE Ref: 08-21-88654 – NC 01 and dated 27/10/2021 shall be submitted to and approved in writing the local planning authority. The details as approved by the local planning authority shall be fully installed before the development is occupied and thereafter be permanently retained.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

32. Prior to occupation of any unit a noise assessment shall be carried out to confirm the unit performs in accordance with the approved noise targets. Any additional steps required to mitigate noise shall be detailed and implemented, as necessary. The post completion noise assessment shall be submitted to and approved in writing by the local planning authority. The details as approved shall thereafter be permanently retained.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

33. No externally located plant or equipment shall be installed or operated without the prior written approval of the Local Planning Authority.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

34. The use hereby permitted, or the operation of any plant, machinery equipment or building services plant, shall not commence until an assessment of the cumulative acoustic impact arising from the operation all the plant, machinery or equipment has been submitted to and approved in writing by the local planning authority.

The assessment of the acoustic impact shall be undertaken in accordance with BS 4142: 2014 (or subsequent superseding equivalent) and current best practice and shall include a scheme of attenuation measures to ensure the cumulative rating level of noise emitted from the proposed plant, machinery equipment or building services plant is 5 dB less than background.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

35. The use hereby permitted, or the operation of any plant, machinery, equipment or building services plant, shall not commence until a post-installation noise assessment has been carried out to confirm compliance with the noise criteria. The scheme shall be implemented in accordance with the approved details and attenuation measures, and they shall be permanently retained and maintained in working order for the duration of the use and their operation.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

36. No activities, including loading or unloading, shall be undertaken in the open air after 23:00 hours or before 07:00 hours on any day.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

37. No externally located storage of any materials or waste is permitted.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

38. Prior to occupation a lighting scheme must be submitted for the approval of the Local Planning Authority in accordance with the Institute of Lighting Professional's Guidance notes for the reduction of obstructive light. The scheme must be designed by a suitably qualified person in accordance with the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20.

Before commencement of operation of the approved lighting scheme the applicant shall appoint a suitably qualified member of the institute of lighting professionals (ILP) to validate that the lighting scheme as installed conforms to the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20 the validation report shall be submitted to and approved by the local planning authority and the approved details and attenuation measures shall be permanently retained and maintained in working order for the duration of the use and their operation.

Reason: To protect the amenities of adjoining occupiers and the surrounding area (Policy CS10 Sustainable Development of RBBC Core Strategy (2014)).

39. The development shall not be occupied until a scheme demonstrating compliance with the principles of 'Secured by Design' has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be completed before the occupation of the development hereby permitted and shall be permanently maintained as such thereafter.

Reason: To ensure that the development provides a secure environment for future residents in accordance with Policy DES1 of the Reigate & Banstead Development Management Plan 2019.

40. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), and Use Classes Order (or any Order revoking and re-enacting that Order with or without modification) the units hereby approved shall be occupied for purposes falling within Use Class E (g) (iii) (any industrial processes) B2 (general industrial and/or B8 (storage and/or distribution) only and shall not be subdivided or used within any other use without the prior written consent of the Local Planning Authority.

Reason: To control the use of the premises in the interests of maintaining an adequate supply of industrial, storage and distribution uses within the designated employment area and with respect to the adequacy of parking provision and potential impacts on the surrounding highway network.

INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at www.firesprinklers.info.
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : Climate Change Information.
3. You are advised that the Council will expect the following measures to be included in the above CMS condition to control noise, pollution and parking:
 - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
 - (c) Deliveries should only be received within the hours detailed in (a) above;
 - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - (e) There should be no burning on site;
 - (f) Only minimal security lighting should be used outside the hours stated above; and

(g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - www.ccscheme.org.uk/index.php/site-registration.

4. The applicant is advised that the essential requirements for an acceptable communication plan forming part of the CMS are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
5. The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see: www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice.
6. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
7. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned

- wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
8. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
 9. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
 10. The Highway Authority has no objection to the proposed development, subject to the above conditions but, if it is the applicant's intention to offer any of the roadworks included in the application for adoption as maintainable highways, permission under the Town and Country Planning Act should not be construed as approval to the highway engineering details necessary for inclusion in an Agreement under Section 38 of the Highways Act 1980. Further details about the post-planning adoption of roads may be obtained from the Transportation Development Planning Division of Surrey County Council.
 11. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
 12. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
 13. Network rail - Due to the close proximity of the proposed development to Network Rail's land and the operational railway, Network Rail requests the applicant / developer engages Network Rail's Asset Protection and Optimisation (ASPRO) team via AssetProtectionLondonSouthEast@networkrail.co.uk prior to works commencing.
 14. Thames Water - A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would

expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer.

15. Thames Water - The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide “working near our assets” to ensure your workings are in line with the necessary processes you need to follow if you’re considering working above or near our pipes or other structures

16. Gatwick Safeguarding - Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks notice. For crane queries/applications please email: lgwcranes@gatwickairport.com

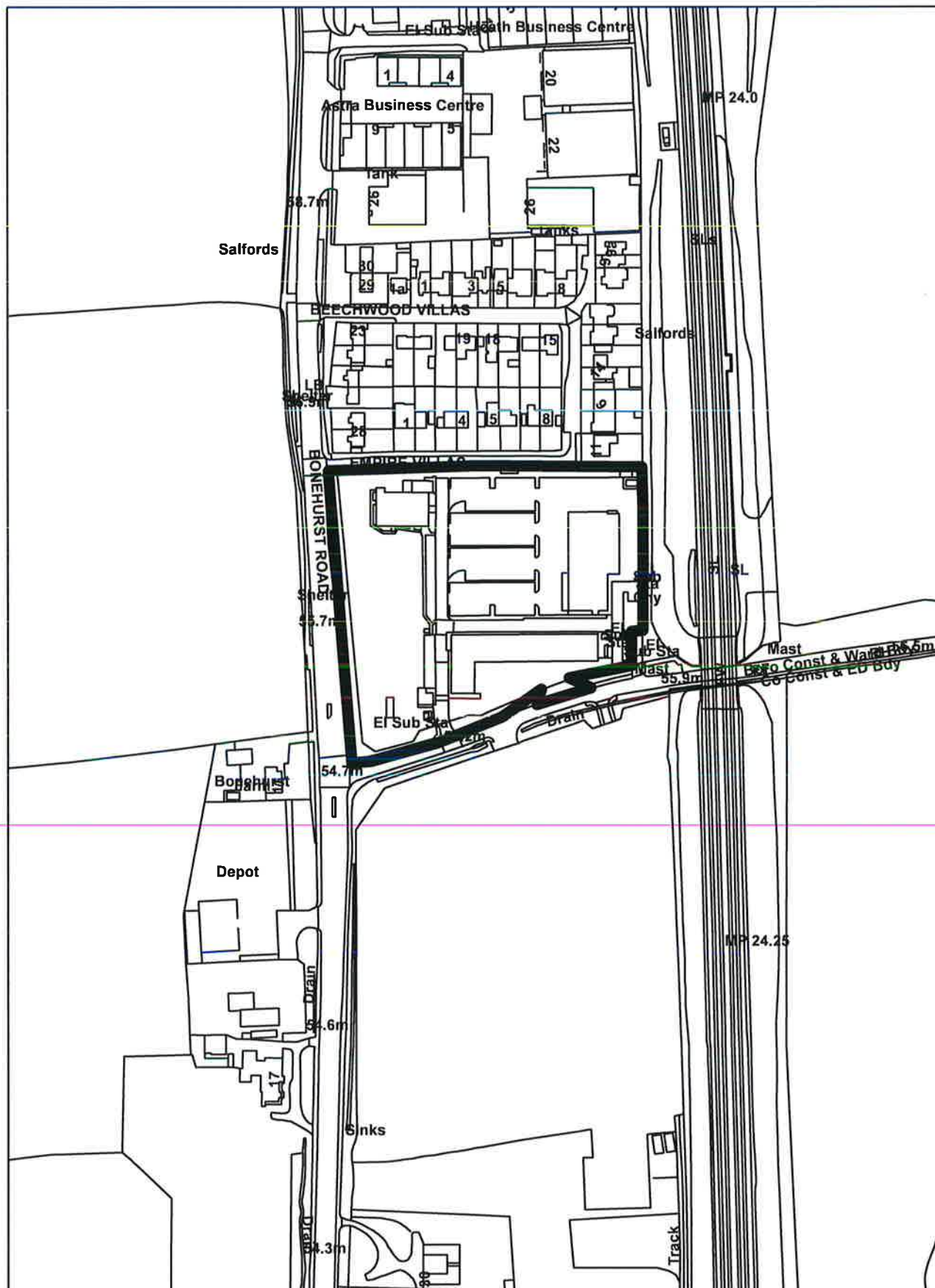
REASON FOR PERMISSION

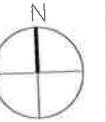
The development hereby permitted has been assessed against the relevant development plan policies as set out in the report and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

21/03303/F - Titan House, Crossoak Lane, Salfords





KEY:

- Planning Application Boundary
16,351 m² / 4.04 acres / 1.635 hectare
- Proposed fence 2.4m high
- ss Proposed substation location
- ref Proposed refuse enclosure
- EV 20% EV charging bays (18no.)
- EV 20% potential future EV charging bays (18no.)

GROSS INTERNAL AREAS:

Unit A - 5,091.5 sqm (54,800 sqft)

Warehouse	4,515.5 sqm (48,600 sqft)
incl office undercroft	
Offices on 1st floor	288 sqm (3,100 sqft)
Offices on 2nd floor	288 sqm (3,100 sqft)
Parking incl disabled	65 spaces
Cycle spaces	20 spaces

Unit B - 2,713.5 sqm (29,200 sqft)

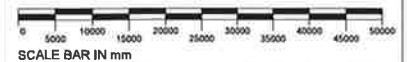
Warehouse	2,397 sqm (25,800 sqft)
incl office undercroft	
Offices on 1st floor	158.25 sqm (1,700 sqft)
Offices on 2nd floor	158.25 sqm (1,700 sqft)
Parking incl disabled	23 spaces
Cycle spaces	14 spaces

TOTAL - 7,805 sqm (84,000 sqft)

Parking incl disabled	88 spaces
Cycle spaces	34 spaces

P3	cycle parking numbers shown on plan	05.05.2022	WR	WRA
P2	14 cycle spaces added, EV charge points shown	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

PLANNING



WARREN ROSING ARCHITECTS
ARCHITECTURE | INTERIORS | DESIGN
Unit 18a Hillgate Place, 18-20 Batham Hill,
London, SW12 9ER
t 020 7993 8247; info@wr-architects.co.uk; www.wr-architects.co.uk

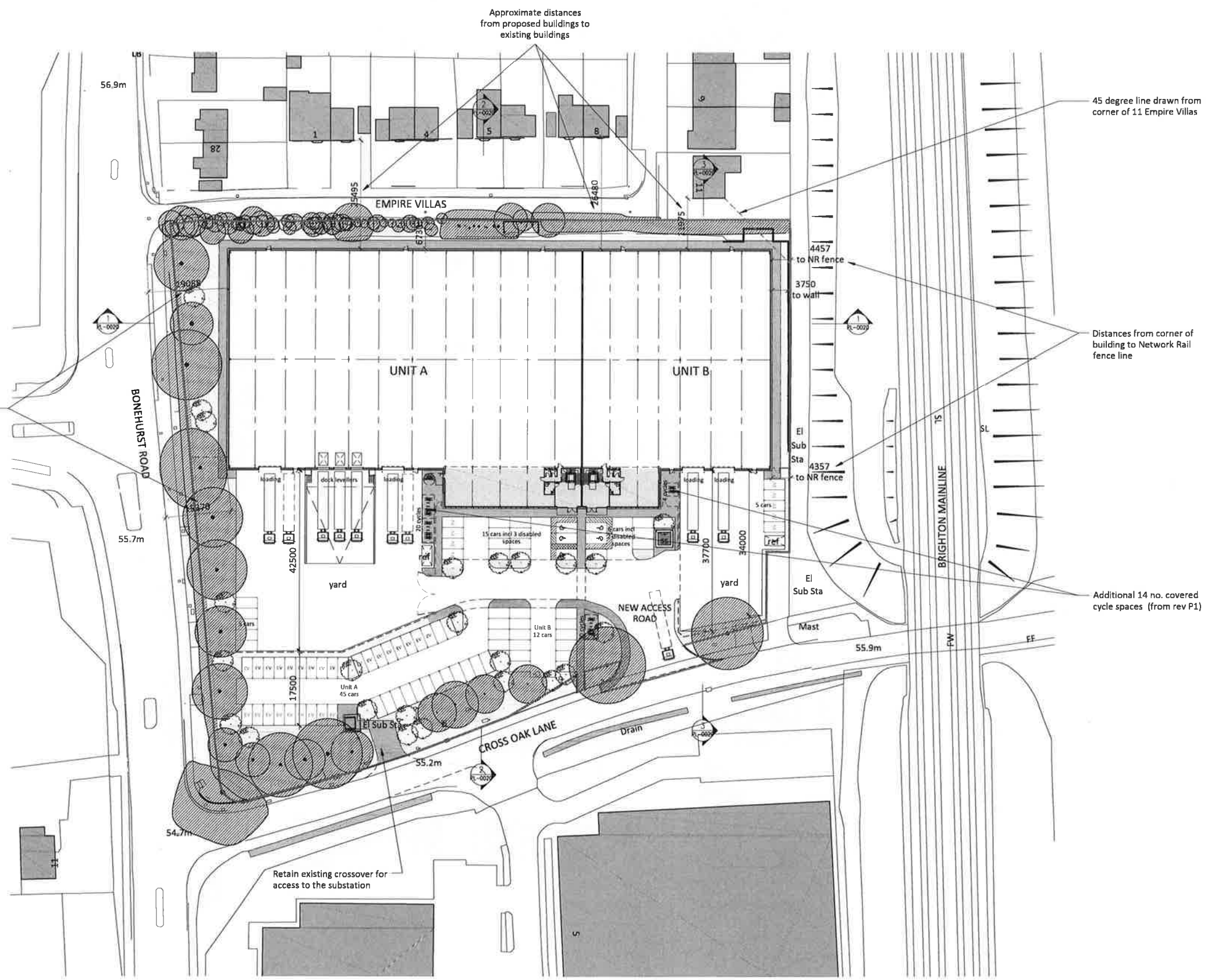
**Cross Oak Lane
Redhill RH1 5EX**

Proposed Site Plan

Drawing No.: 2105-PL-0001 Rev: P3

scale: 1:1000 @ A3 10 December 2021

do not scale this drawing; check all dimensions and areas on site
copyright of pocket door ltd trading as 'warren rosing architects'



Approximate distances from corner of building to kerb line

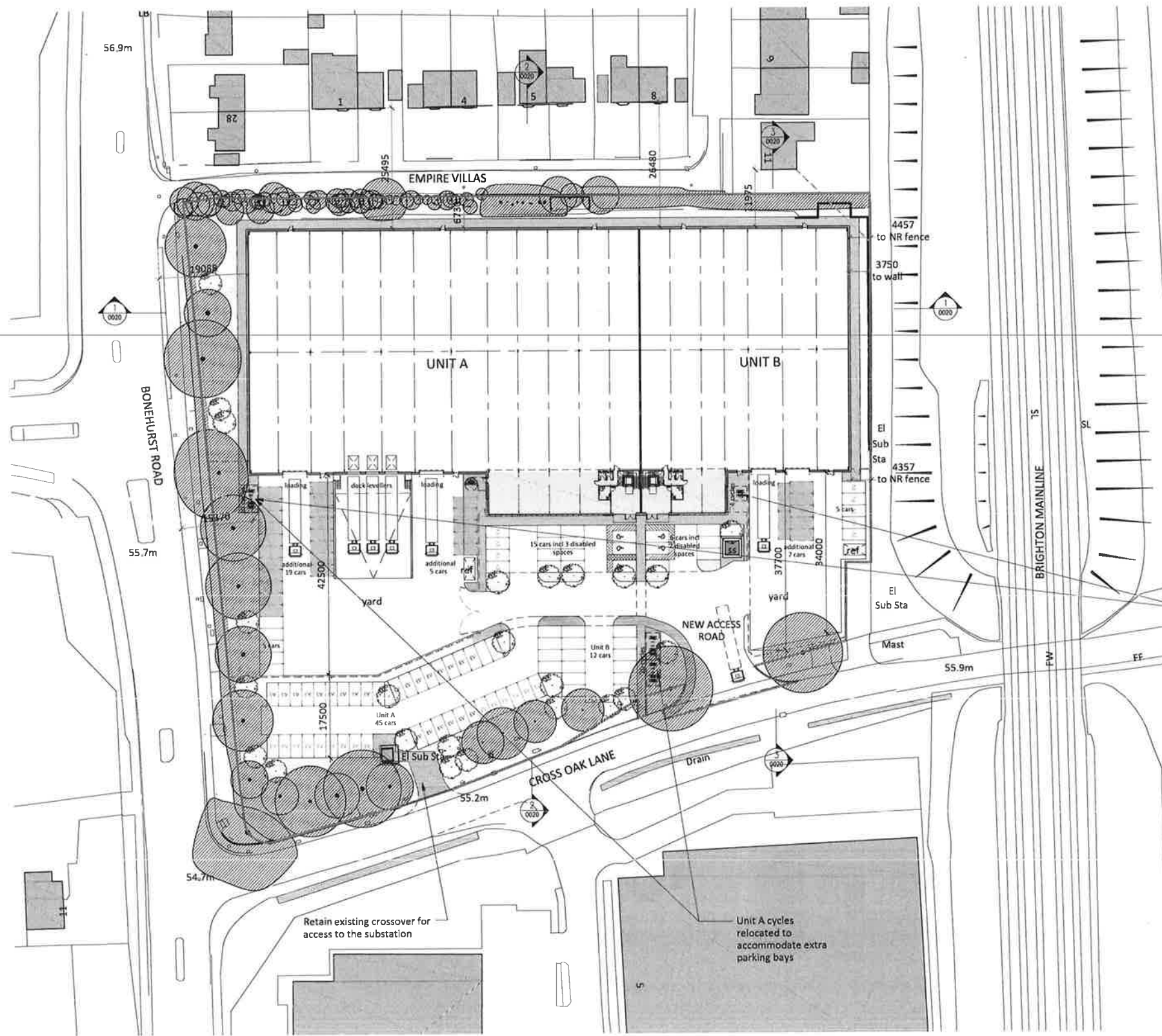
Approximate distances from proposed buildings to existing buildings

45 degree line drawn from corner of 11 Empire Villas

Distances from corner of building to Network Rail fence line

Additional 14 no. covered cycle spaces (from rev P1)

Retain existing crossover for access to the substation



- KEY:**
- Planning Application Boundary
16,351 m² / 4.04 acres / 1.635 hectare
 - - - Proposed fence 2.4m high
 - ss Proposed substation location
 - ref Proposed refuse enclosure
 - EV 20% EV charging bays (24no.)
 - EV 20% potential future EV charging bays (24no.)
 - Additional parking bays (allow for 20% EV charge bays [6no.] + 20% future EV charge bays [6no.])

PARKING & CYCLES

Unit A -	
Parking incl disabled	89 spaces
Cycle spaces	20 spaces
Unit B -	
Parking incl disabled	30 spaces
Cycle spaces	14 spaces
TOTAL	
Parking incl disabled	119 spaces
Cycle spaces	34 spaces

P3	cycle numbers added, extra EV charge points shown	05.05.2022	WR	WRA
P2	14 cycle spaces added, EV charge points shown	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

PLANNING

SCALE BAR IN mm

WARREN ROSING ARCHITECTS
 ARCHITECTURE | INTERIORS | DESIGN
 Unit 18a Hillgate Place, 18-20 Baiham Hill,
 London, SW12 9ER
 020 7993 8247; info@wr-architects.co.uk; www.wr-architects.co.uk

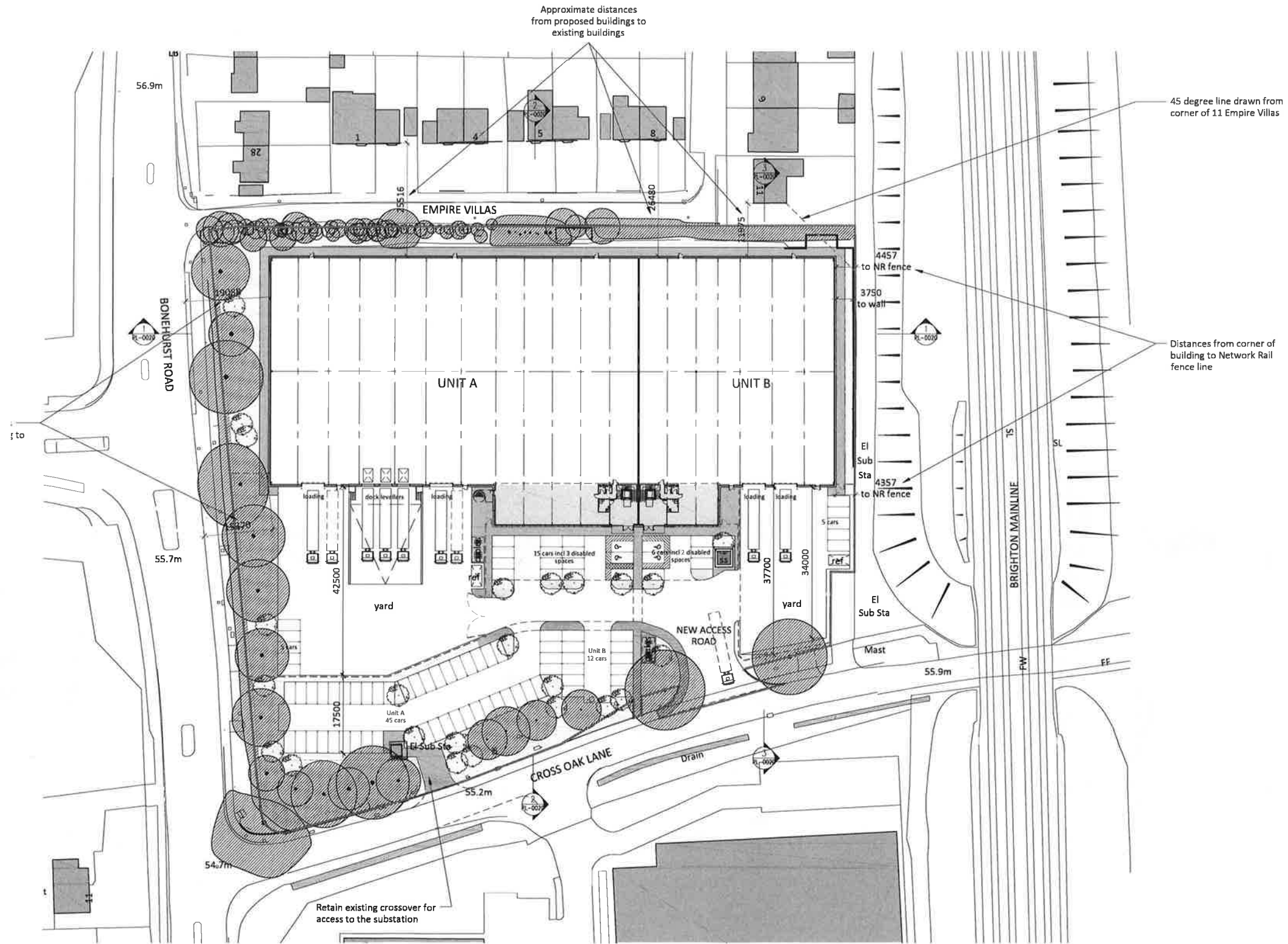
**Cross Oak Lane
 Redhill RH1 5EX**

**Proposed Site Plan with Parking
 Layout for B2 / E (g) (iii) Use**

Drawing No.: 2105-PL-0003 Rev: P3
 scale: 1:1000 @ A3 10 December 2021

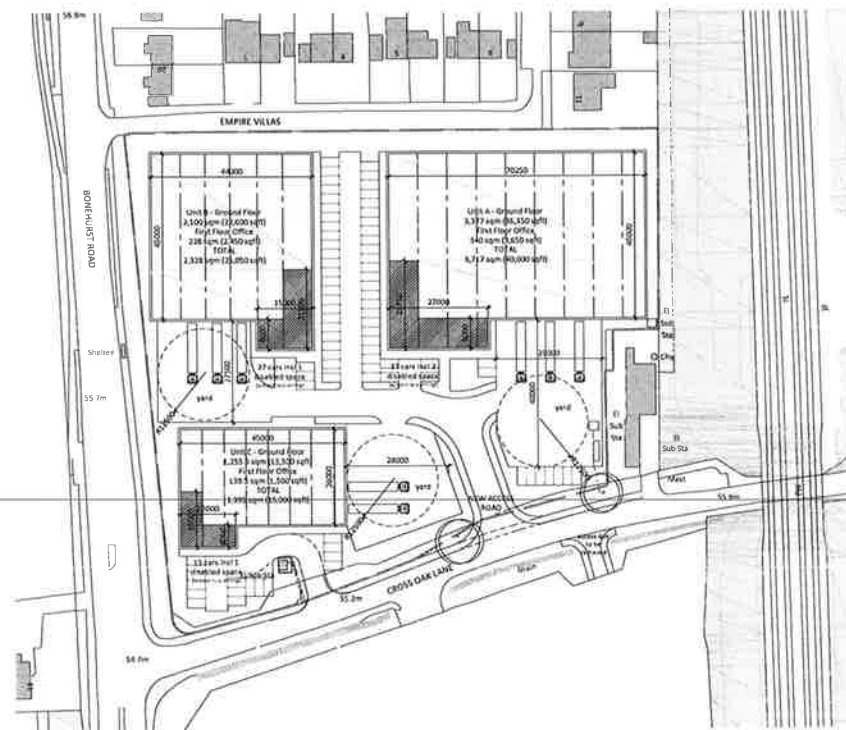
Do not scale this drawing, check all dimensions and areas on site
 copyright of pocket door ltd trading as 'warren rosing architects'

SITE LAYOUT OPTIONS STUDY
 SUBMITTED SITE LAYOUT FOR REFERENCE

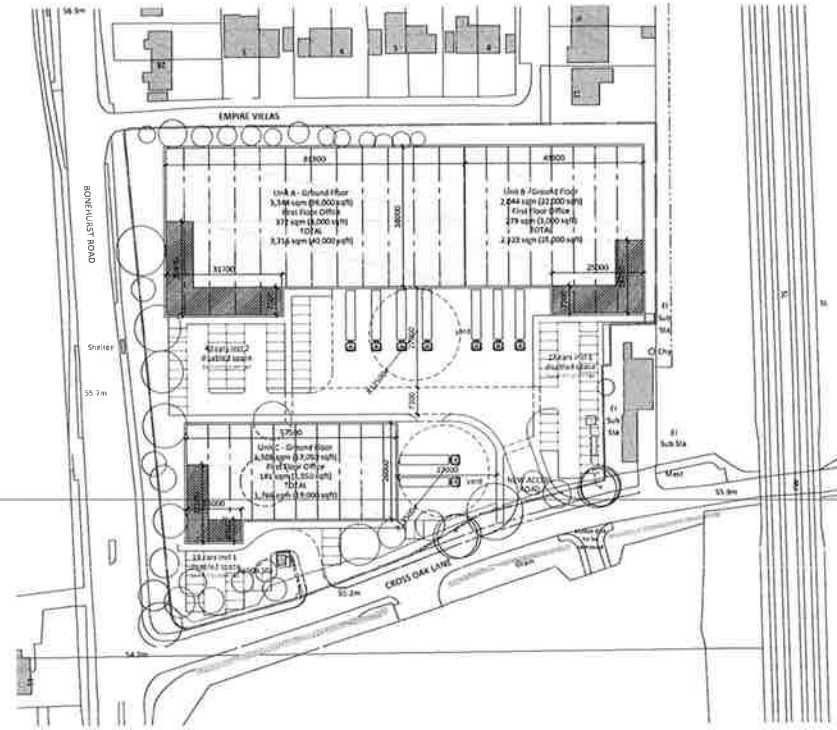


SITE LAYOUT OPTIONS STUDY

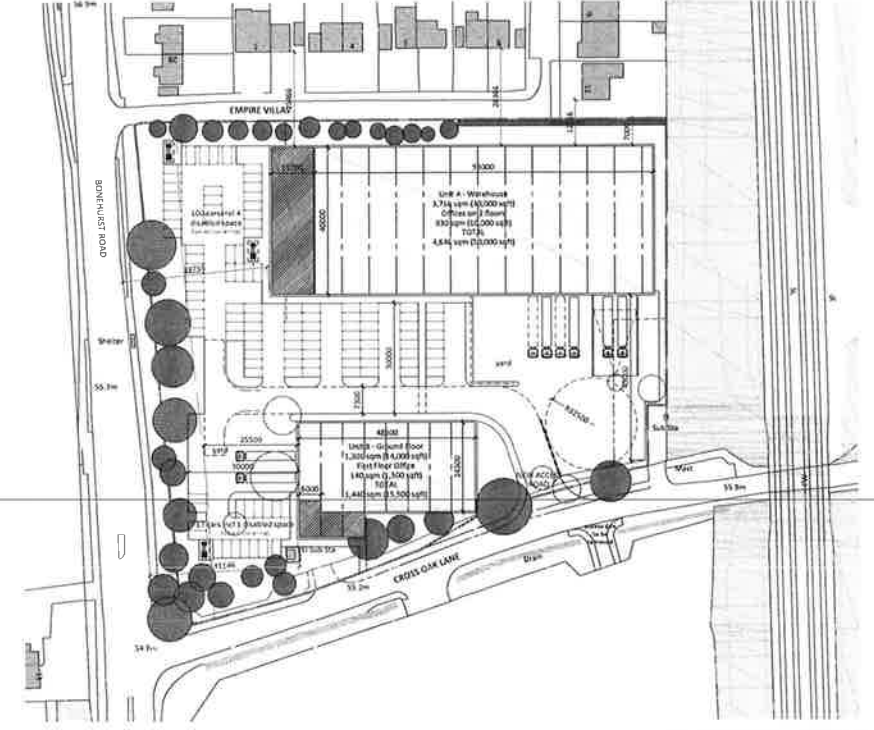
EARLIER SITE LAYOUT STUDIES PRIOR TO SUBMITTING THE PLANNING APPLICATION



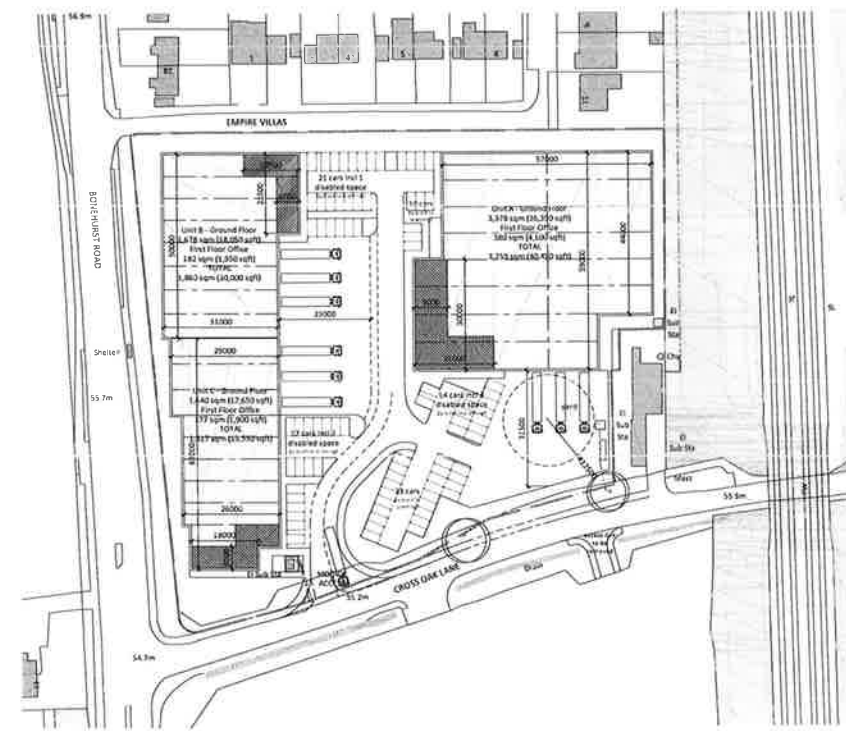
09 February 2021: THREE BUILDINGS - 3 UNITS, 2 ACCESS ROADS, GAP TO EMPIRE VILLAS & CLOSER TO BOUNDARY, LOSS OF TREES TO CROSS OAK LANE & BONEHURST ROAD



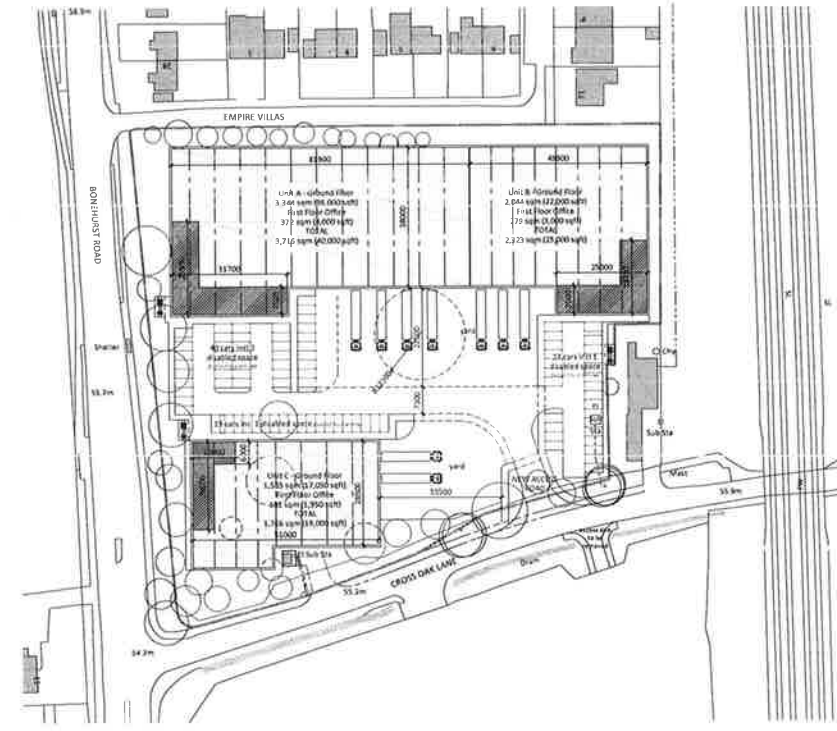
24 February 2021: TWO BUILDINGS - 3 UNITS, 2 ACCESS ROADS, CLOSER TO EMPIRE VILLAS BOUNDARY
LOSS OF TREES TO CROSS OAK LANE



09 July 2021: TWO BUILDINGS - 2 UNITS, NEW ACCESS ROAD, CLOSER TO EMPIRE VILLAS BOUNDARY
RETAIN MORE TO CROSS OAK LANE, CAR PARK ADJACENT EMPIRE VILLAS



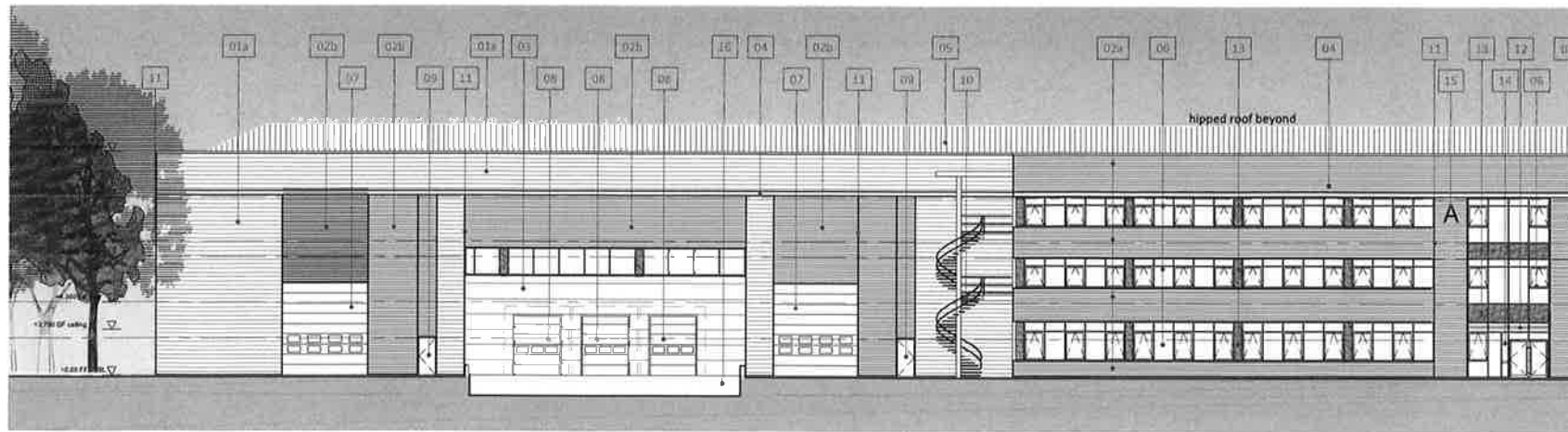
11 February 2021: TWO BUILDINGS - 3 UNITS, RETAIN EXISTING ACCESS, GAP TO EMPIRE VILLAS & CLOSER TO BOUNDARY, LOSS OF TREES TO CROSS OAK LANE & BONEHURST ROAD



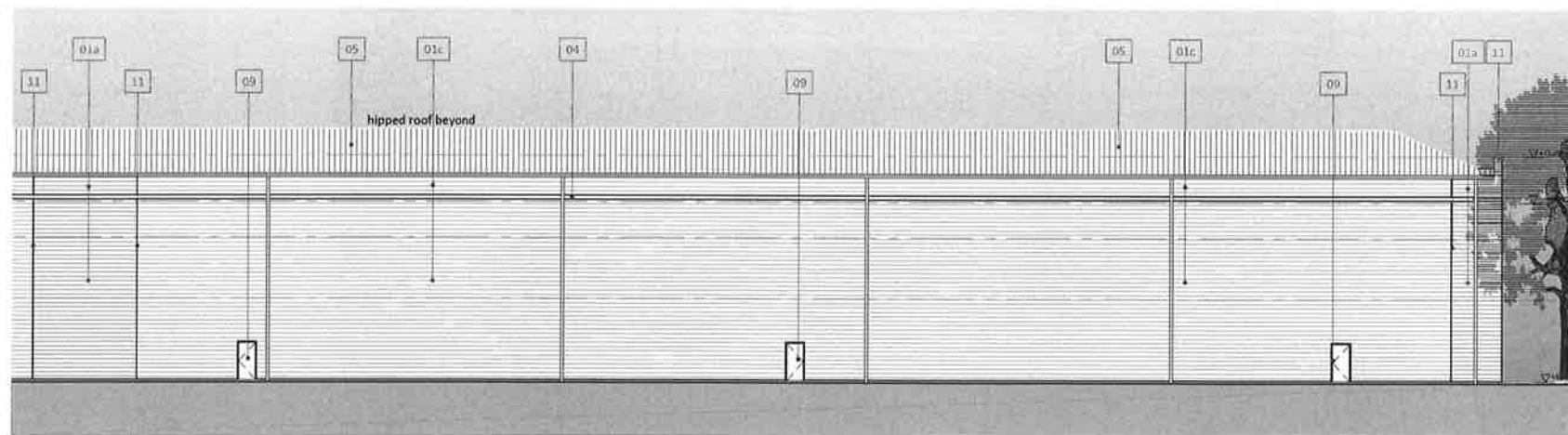
18 March 2021: TWO BUILDINGS - 3 UNITS, NEW ACCESS ROAD, CLOSER TO EMPIRE VILLAS BOUNDARY, RETAIN MOST OF THE TREES TO CROSS OAK LANE & BONEHURST ROAD



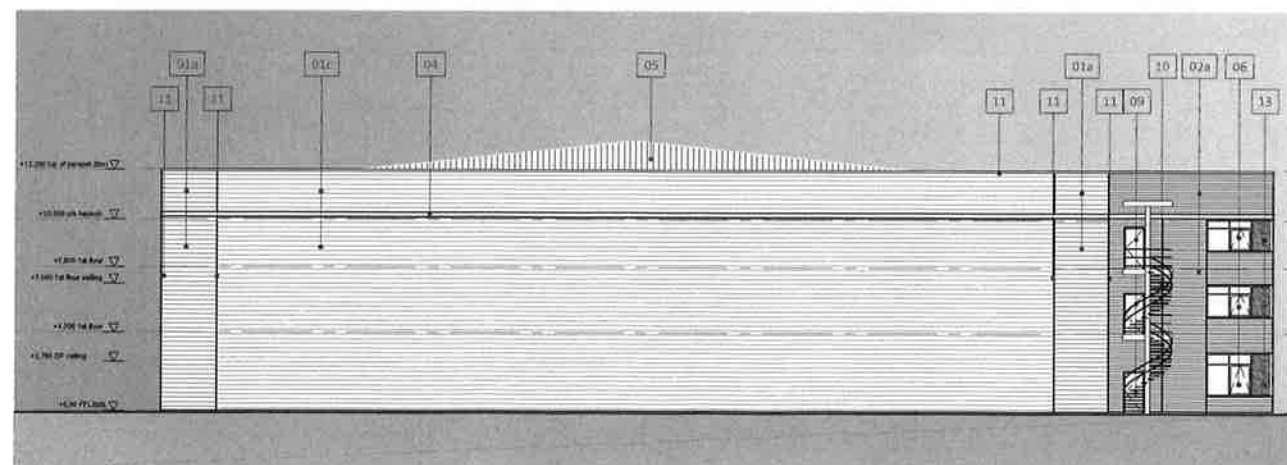
09 July 2021: THREE BUILDINGS - 3 UNITS, NEW ACCESS ROAD, GAP TO EMPIRE VILLAS &, LOSS OF FEWER TREES TO CROSS OAK LANE



SOUTH ELEVATION - UNIT A



NORTH ELEVATION - UNIT A

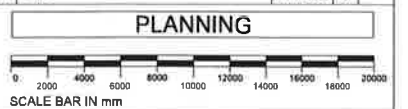


WEST ELEVATION - UNIT A

MATERIALS KEY:

- 01a Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Orion (tbc)
- 01b Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Atlantis (tbc)
- 01c Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Aquarius (tbc)
- 01d Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Sirius (tbc)
- 02a Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zeus (tbc)
- 02b Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Atlantis (tbc)
- 03 Wall cladding: Eurobond Europanel composite cladding cladding - colour Prisma Atlantis (tbc)
- 04 Feature channel flashings: Colour tbc
- 05 Roof cladding: Profiled built up steel cladding - colour: Goosewing Grey
- 06 Strip windows & curtain walling: Frame Colour: Anthracite (RAL 7016)
- 07 Level loading doors: Colour tbc
- 08 Dock leveller doors: Colour tbc
- 09 Metal escape & personnel door: Colour to match adjacent cladding
- 10 Galvanized steel spiral escape stair and roof
- 11 Metal flashings: Colour tbc
- 12 Glazed entrance canopy on ppc metal supports
- 13 Look-a-like glazing spandrel panels
- 14 Metal spandrel panel to match glazing with letter box
- 15 Unit & door numbers
- 16 Concrete dock retaining walls

P4	Sloped roof shown	23.06.2022	WR	WRA
P3	Building height lowered by 0.5m	16.06.2022	WR	WRA
P2	Rear parapet dropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth



WARREN ROSING ARCHITECTS
 ARCHITECTURE | INTERIORS | DESIGN
 Unit 18a Hillgate Place, 18-20 Balham Hill,
 London, SW12 9ER
 t: 020 7993 8247; info@wr-architects.co.uk; www.wr-architects.co.uk

**Cross Oak Lane
 Redhill RH1 5EX**

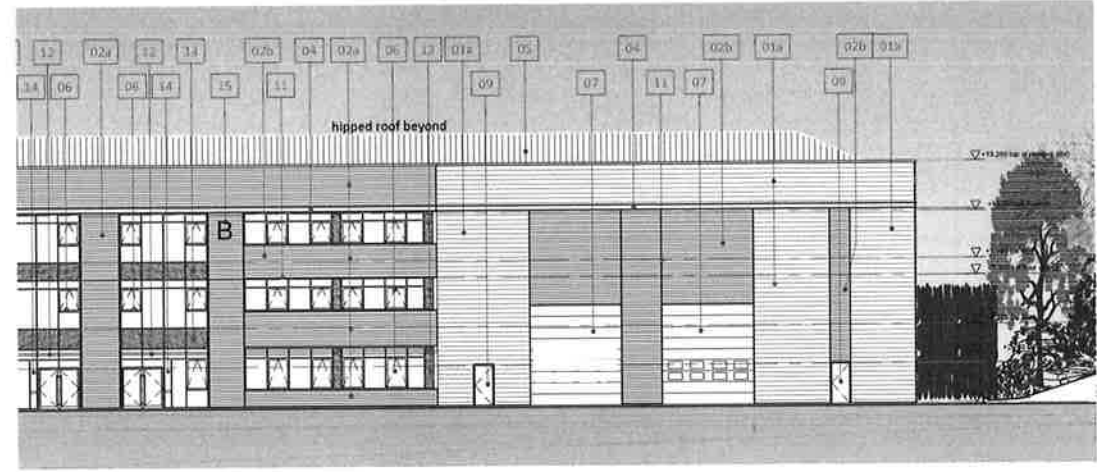
**Unit A
 Elevations**

Drawing No.: 2105-PL-1015 Rev: P4
 scale: 1:400 @ A3 / 1:200 @ A1 10 December 2021

do not scale this drawing; check all dimensions and areas on site
 copyright of pocket door ltd trading as 'warren rosing architects'

WARREN ROSING ARCHITECTS
 ARCHITECTURE | INTERIORS | DESIGN
 Unit 18a Hillgate Place, 18-20 Balham Hill,
 London, SW12 9ER
 020 7993 8247, info@wr-architects.co.uk

SCALE BAR IN mm



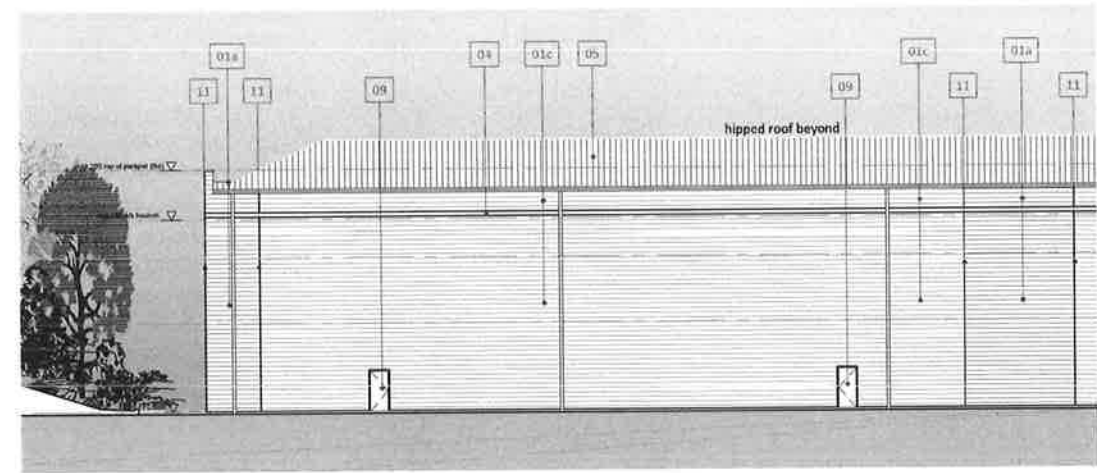
SOUTH ELEVATION - UNIT B

MATERIALS KEY:

- △ 01a up steel cladding
- △ 01c up steel cladding
- △ 02a up steel cladding
- △ 02b up steel cladding
- △ 02c up steel cladding
- △ 03 up steel cladding
- △ 04 up steel cladding
- △ 05 up steel cladding
- △ 06 up steel cladding
- △ 07 up steel cladding
- △ 08 up steel cladding
- △ 09 up steel cladding
- △ 10 up steel cladding
- △ 11 up steel cladding
- △ 12 up steel cladding
- △ 13 up steel cladding
- △ 14 up steel cladding
- △ 15 up steel cladding
- △ 16 up steel cladding

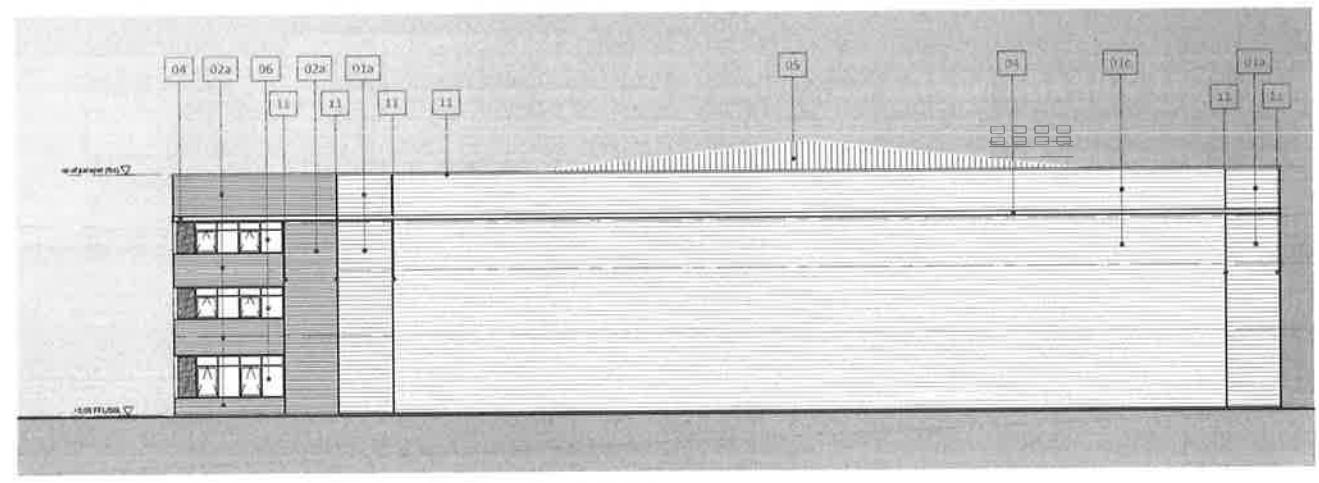
up steel cladding - colour Prisma Atlantis (tbc)

15
16 retaining all



NORTH ELEVATION- UNIT B

- 01a Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01b Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01c Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01d Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01e Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01f Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01g Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01h Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01i Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01j Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01k Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01l Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01m Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01n Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01o Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01p Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01q Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01r Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01s Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01t Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01u Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01v Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01w Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01x Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01y Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 01z Wall cladding: Horizontal trapezoidal profiled built up steel cladding - colour Prisma Cron (tbc)
- 02a Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02b Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02c Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02d Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02e Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02f Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02g Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02h Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02i Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02j Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02k Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02l Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02m Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02n Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02o Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02p Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02q Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02r Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02s Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02t Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02u Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02v Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02w Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02x Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02y Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 02z Wall cladding: Horizontal sinusoidal profiled built up steel cladding - colour Prisma Zena (tbc)
- 03 Wall cladding: European composite cladding - colour Prisma Atlantis (tbc)
- 04 Feature channel flashings: Colour tbc
- 05 Roof cladding: Profiled built up steel cladding - colour Prisma Atlantis (tbc)
- 06 Strip windows & curtain walling: Frames Colour: Anthracite (RAL 7016)
- 07 Level loading doors: Colour tbc
- 08 Dock leveler doors: Colour tbc
- 09 Metal escape & personnel door: Colour to match adjacent cladding
- 10 Galvanized steel spiral escape stair and roof
- 11 Metal flashings: Colour tbc
- 12 Glazed entrance canopy on ppc metal supports
- 13 Look-a-like glazing spandrel panels
- 14 Metal spandrel panel to match glazing with letter box
- Unit & door numbers
- W s Concrete dock



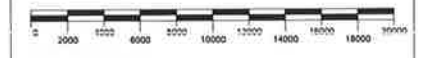
EAST ELEVATION - UNIT B

PLANNING

**Cross Oak Lane
 Redhill RH1 5EX
 Unit B
 Elevations**

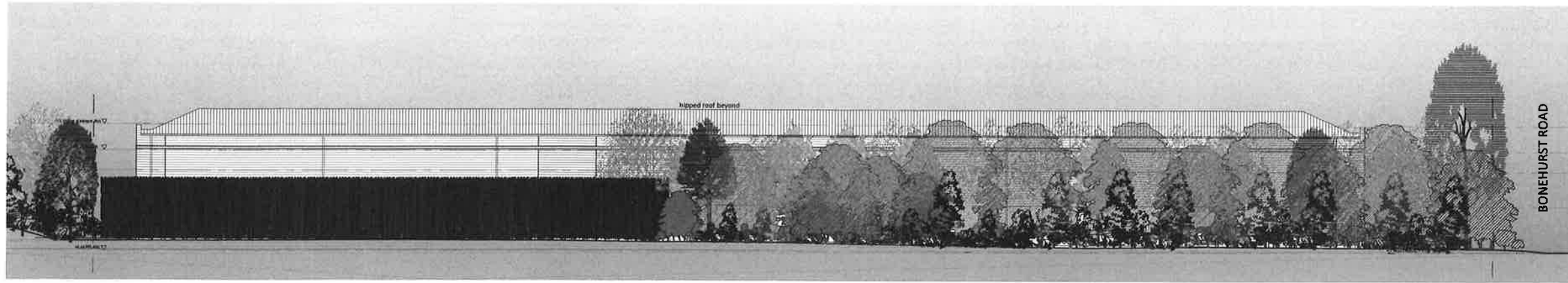
Drawing No.: 2105-PL- 1016 Rev: P4

P4	Sloped roof shown	23.06.2022	WR	WRA
P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet dropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	6d.mm.yyyy		

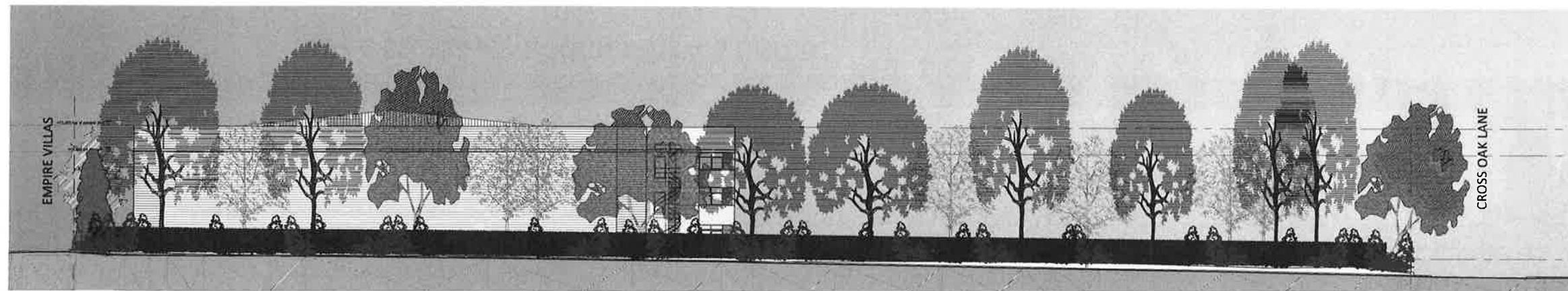


www.wr-architects.co.uk

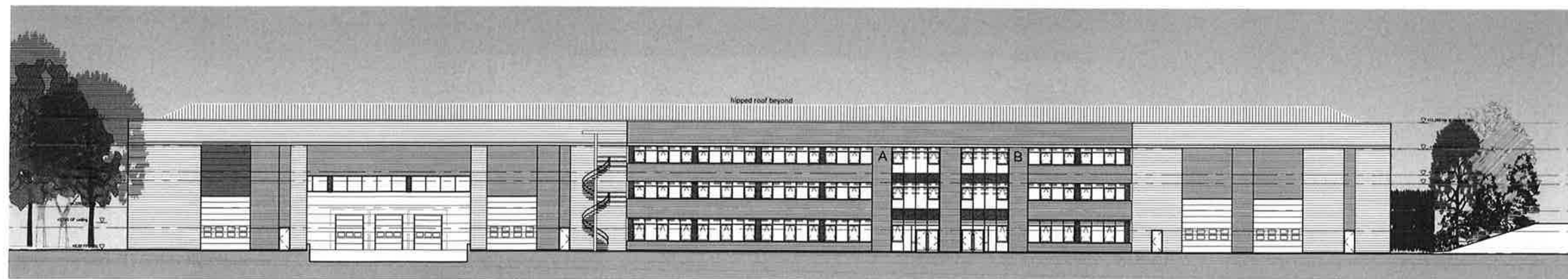
Do not scale this drawing, check all dimensions and areas on site copyright of pocket door ltd trading as 'warren rosing architects'



1. SITE BOUNDARY ELEVATION - NORTH BOUNDARY (EMPIRE VILLAS)



2. SITE BOUNDARY ELEVATION - WEST BOUNDARY (BONEHURST ROAD)



3. FRONT ELEVATION UNITS A & B

P4	Slip at roof shown	23.05.2022	WR	WRA
P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet dropped and outboard gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

PLANNING

SCALE BAR IN mm

WARREN ROSING ARCHITECTS
 ARCHITECTURE | INTERIORS | DESIGN
 Unit 18a Hillgate Place, 18-20 Balham Hill,
 London, SW12 9ER
 T 020 7993 8247; info@wr-architects.co.uk; www.wr-architects.co.uk

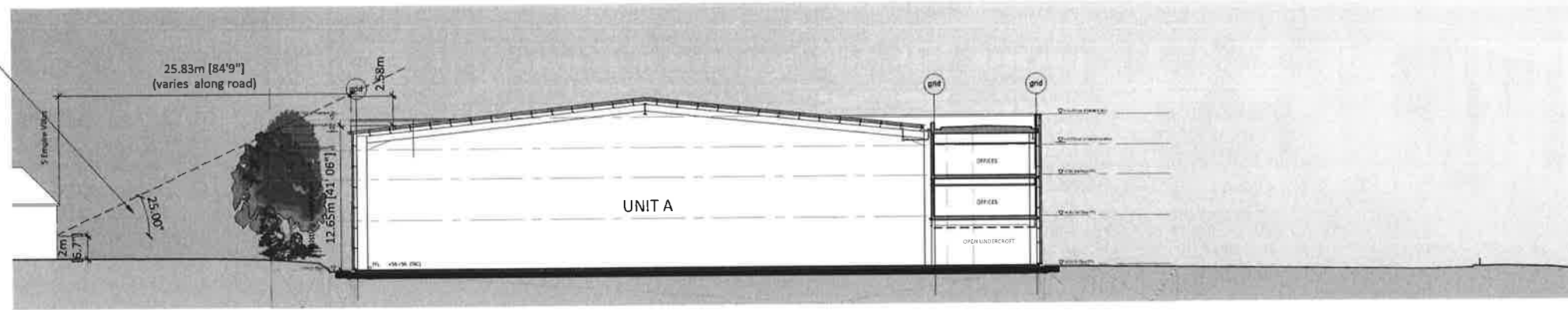
Cross Oak Lane
Redhill RH1 5EX

Site Boundary & Full Front Elevations

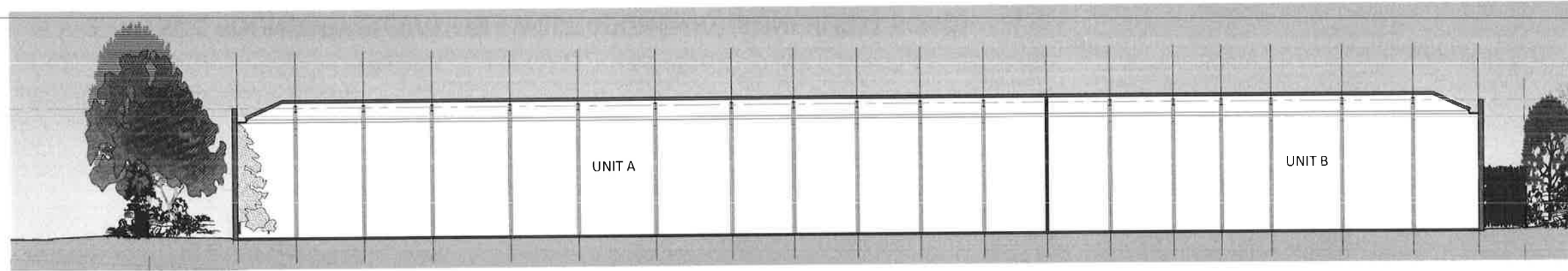
Drawing No.: 2105-PL-0021 Rev: P4
 scale: 1:500 @ A3 / 1:250 @ A1 10 December 2021

Not to scale. This drawing, check all dimensions and areas on site copyright of pocket door ltd trading as 'warren rosing architects'

Sight line as set out in Reigate and Banstead pre-application letter - 25 degrees at 2 metres above ground level where there are windows

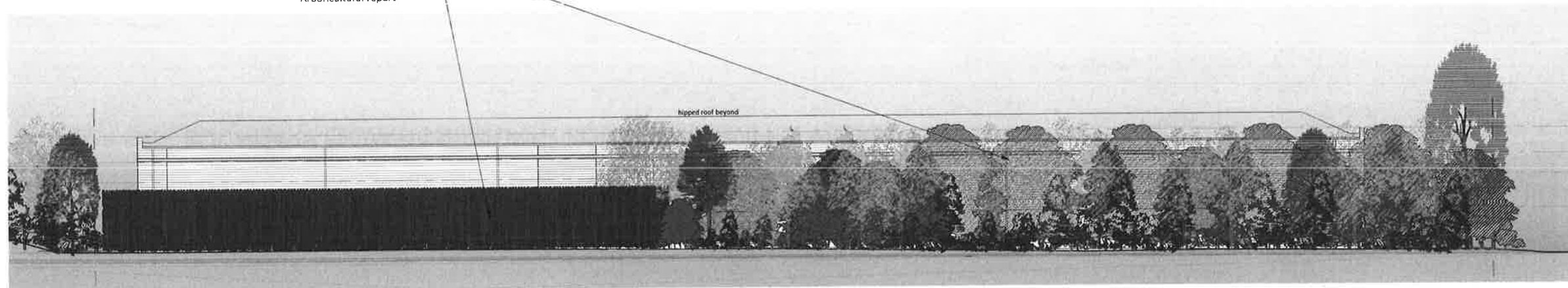


1. CROSS SECTION THROUGH UNIT A & 5 EMPIRE VILLAS



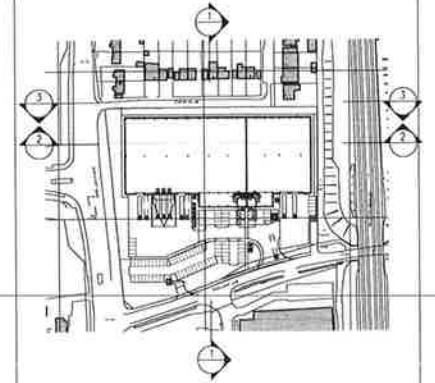
2. LONG SECTION THROUGH UNIT A & UNIT B

Note: varying height of existing landscaping based on information from the Arboricultural report



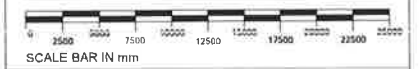
3. SITE BOUNDARY ELEVATION - NORTH BOUNDARY (EMPIRE VILLAS)

KEY PLAN:



C	Building height lowered by 0.5m	23.06.2022	WR	WRA
A	Cladding changes to rear elevation to break scale	16.03.2022	WR	WR
Rev	Notes	dd.mm.yyyy	By	Auth

PRELIMINARY



WARREN ROSING ARCHITECTS
ARCHITECTURE | INTERIORS | DESIGN
Unit 18a Hillgate Place, 18-20 Baham Hill,
London, SW12 9ER
t 020 7993 8247; info@wr-architects.co.uk; www.wr-architects.co.uk

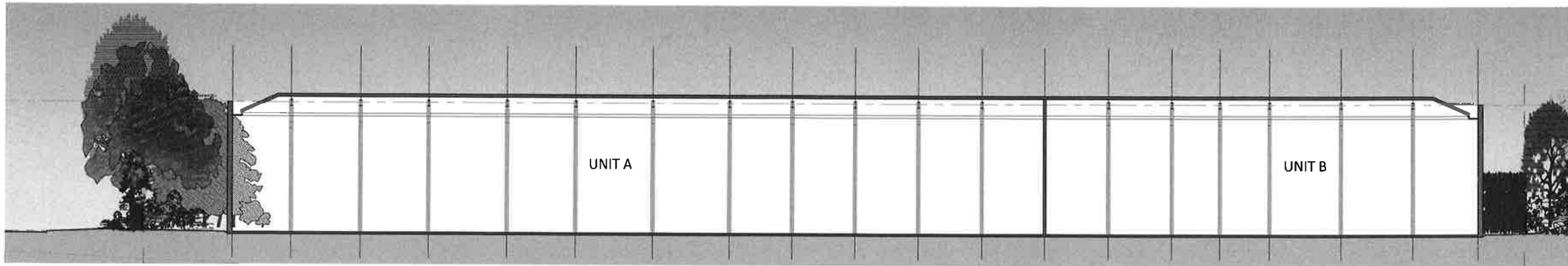
Cross Oak Lane
Redhill RH1 5EX

Site Sections & Boundary Elev.
to Empire Villas (no parapet)

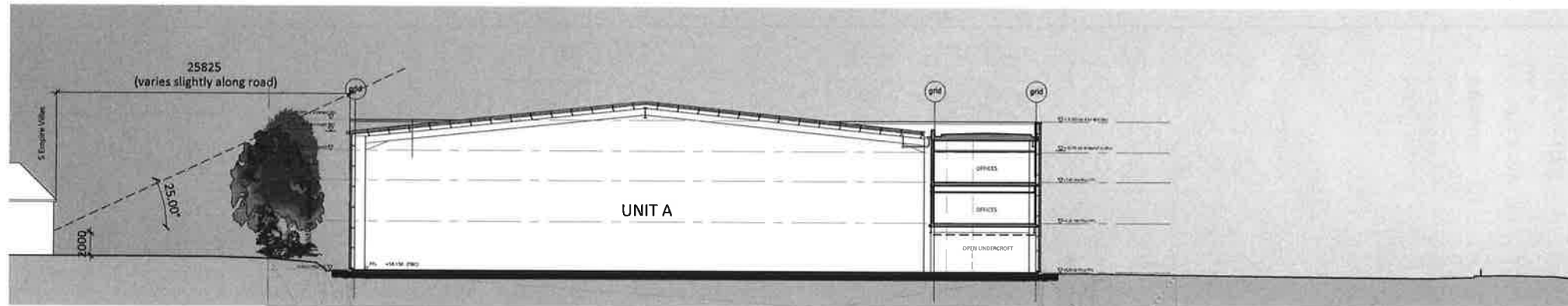
Drawing No.: 2105-SK-0023 Rev. C

scale: 1:500 @ A3 / 1:250 @ A1 07 March 2022

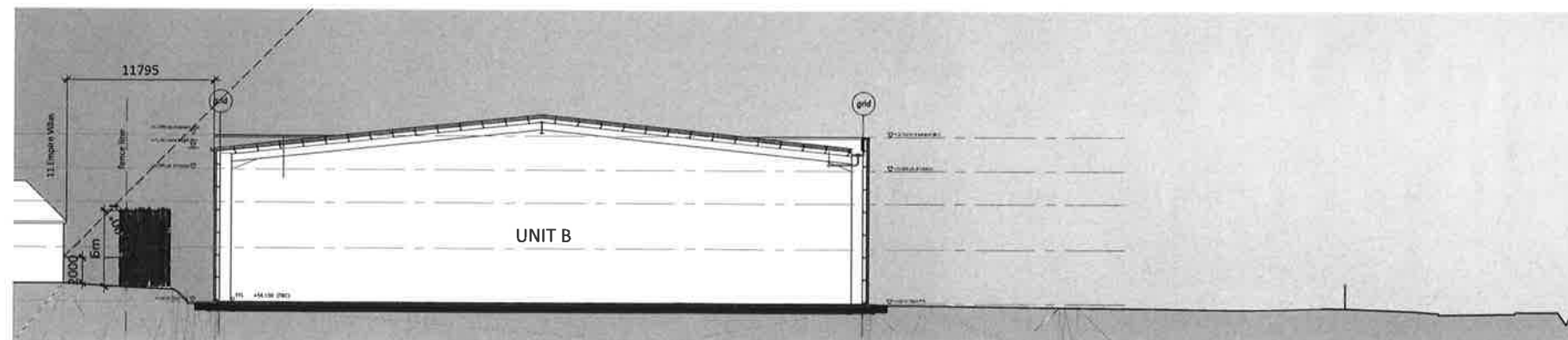
do not scale this drawing; check all dimensions and areas on site
copyright of pocket door ltd trading as warren rosing architects



1. LONG SECTION THROUGH UNIT A & UNIT B



2. CROSS SECTION THROUGH UNIT A & 5 EMPIRE VILLAS



3. CROSS SECTION THROUGH UNIT B & 11 EMPIRE VILLAS

P3	Building height lowered by 0.5m	16.05.2022	WR	WRA
P2	Rear parapet dropped and outward gutter added	04.04.2022	WR	WRA
P1	Issued for Planning Approval	10.12.2021	WR	WRA
Rev	Notes	dd.mm.yyyy	By	Auth

PLANNING



WARREN ROSING ARCHITECTS
ARCHITECTURE | INTERIORS | DESIGN
Unit 16a Hillgate Place, 16-20 Balham Hill,
London, SW 12 9ER
t 020 7993 8247; info@warrenrosing.co.uk; www.wr-architects.co.uk

Cross Oak Lane
Redhill RH1 5EX
Site Sections

Drawing No.: 2105-PL-0020 Rev: P3
scale: 1:500 @ A3 / 1:250 @ A1 10 December 2021

do not scale this drawing, check all dimensions and areas on site
copyright of pocket door ltd trading as 'warren rosing architects'

for design purposes only